

Categorical Exclusion, Level 2

CLE-28-2.05 Castleberry Court to Interstate 275

PID 82140

November 13, 2009



INNOVATIVE FACILITY AND INFRASTRUCTURE DESIGN

**Categorical Exclusion, Level 2
for
CLE-28-2.05 Castleberry Court to Interstate 275
PID 82140**

November 13, 2009



Prepared for:

**Clermont County Transportation Improvement District
175 East Main Street
Suite 150
Batavia, Ohio 45103**

**Prepared By:
LJB Inc.**

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN _____

CE-2 Form..... 1-15

APPENDICES:

Appendix A – Project Mapping

- **Location Map.....16**
- **USGA Maderia Quadrangle.....17**
- **Existing Aerial.....18**
- **Preferred Alternative.....19**
- **Sole Source Aquifer Map.....20**
- **Public Water System Information.....21**
- **FEMA Flood Zone Designations.....22**
- **Traffic Noise Screening Area.....23**
- **Persons Over 65 Map.....24**
- **Percent Speak English Less Than Well Map.....25**
- **Disabled Persons Map.....26**
- **Households Without Vehicles Map.....27**
- **Percent of Individuals in Poverty.....28**
- **Percent Minorities Map.....29**

Appendix B – Resource Agency Coordination

- **OKI TIP and Long Range Plan..... 30-31**
- **MOA Project Notification Letters..... 32-43**
- **ODNR Comments..... 44-51**
- **U.S. Fish & Wildlife Comments..... 52-55**
- **FPPA Screening Sheet.....56**
- **ODOT Cultural Resources Review..... 57-58**
- **Section 6(f) Website Data.....59**
- **PM2.5 Determination..... 60-62**
- **MSAT Determination.....63**
- **Traffic Noise Determination.....64**
- **ODOT/OES Community Impact Assessment Checklist..... 65-67**
- **401/404 Determination.....68**
- **ESA Screening IOC.....69**
- **Phase I ESA IOC..... 70-71**

Appendix C – Public Involvement Documentation 72-89

Appendix D – Photolog

- **Representative Photos Location Map.....90**
- **Representative Photos..... 91-99**

Appendix E – Project Plan Sheets..... 100-129

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

CATEGORICAL EXCLUSION DOCUMENT

Part I - General Project Identification, Description, and Design Information

Sponsor of the Project: Clermont County Engineer ODOT District: 08
Local Name of the Facility: State Route 28

Program: OKI STP Funding Source: [X] Federal [] State [X] Local [] Private

PROJECT DESCRIPTION:

County and Township: Clermont County, Miami Township
Municipality: City of Milford

Limits of Proposed Work: Castleberry Court intersection east to the I-275 southbound on/off ramp intersection.
Start: Sta. 44 + 00 SR 28 / 150' wide End: Sta. 64 + 00 SR 28 / 210' wide
Total Work Length: 2000 ft or (0.4 mi).

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required? Yes* [] No [X]
If yes, when did FHWA grant a conditional approval for this project? Date: []

*If yes, for CE 2 or CE 3 projects a copy of the approved document must be submitted to FHWA with a request with for final approval of the IMS/IJS.

The CLE-28-2.05 project will provide improvements to 0.4 mile of State Route 28 in the City of Milford, Miami Township, Clermont County, Ohio, from immediately west of Castleberry Court eastward to the Interstate 275 interchange with State Route 28.

Existing Conditions (See page 18 of Appendix A). Existing State Route 28 is a three lane roadway with two directional through lanes and a center two-way-left-turn lane from Castleberry Court east to Old Bank Drive.

This State Route 28 project corridor is zoned as general business and office districts within the city of Milford limits. The project has a minimally rolling topography and is primarily developed, with adjacent land uses that include strip commercial, retail and service uses fronting on State Route 28, and residential developments behind the commercial development.

Several commercial drives are offset from drives on the opposite side of the road. The frequency of these drive offsets creates disorderly turning movements through the corridor, increasing vehicle conflict points contributing to congestion.

(See page I-A for a continuation of this section)

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Project Description (continued from page 1)

Proposed Improvements (See page 19 of Appendix A). The proposed project will upgrade the existing three lane section to a five lane section. The typical section includes four 12 foot wide through lanes with a 12 foot two-way left-turn lane running the extent of the project. Twelve foot lane widths are required due to the functional classification of State Route 28 as a minor arterial with an average daily traffic (ADT) volume of 30,950. Construction is scheduled to begin in April 2011.

The westbound lane that currently tapers out west of McClelland will be extended through the length of the project to the Castleberry Court intersection, where it will become an exclusive right-turn-only lane onto Castleberry Court. The second eastbound through lane east of Old Bank Drive will be extended back 1000 feet to Castleberry Court. The existing eastbound right-turn lane serving the I-275 southbound on-ramp will be extended 650 feet west from its current start east of McClelland Road. A 5.5 foot sidewalk at the back of curb will be added on each side of State Route 28 in this section from the Castleberry Court intersection to the McClelland Road intersection. The project will also include upgrades to the existing traffic signals at Castleberry Court and McClelland Road, modifications to side street approach radii, and access management applications. Continuous street lighting is not proposed along State Route 28. Intersection lighting of State Route 28 will be provided at the Castleberry Court and State Route 28 intersections.

Access management, aimed at limiting left-turn movements will be provided by the construction of a raised median east of Old Bank Road. The project will provide additional access management along the corridor by removing or relocating several drives to eliminate offsets and improve intersection geometry.

To construct these improvements minor strips of permanent and temporary right of way will be acquired from approximately 15 adjacent properties. The project will not require the acquisition of any whole parcels.

The proposed project has been included on the 2030 regional transportation plan developed by the Executive Committee of the Ohio-Kentucky-Indiana (OKI) Regional Council of Governments. The plan was adopted by the OKI Board on June 12, 2008. This project is also part of the regional transportation improvement program developed by the CCTID. The relevant pages from the OKI 2030 Regional Transportation Plan and the 2008-2011 Transportation Improvement Program are provided on pages 30 and 31 in Appendix B.

Adjacent Projects. An interchange modification study (IMS) is being conducted by ODOT District 8 that documents the proposed improvements to the interchange as outlined in the previous safety study. Improvements being proposed for the CLE-28-2.05 project were investigated as part of the ODOT District 8 IMS for the I-275 interchange. ODOT intends to begin construction of the proposed interchange project by summer of 2012.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

PURPOSE AND NEED FOR THE PROJECT:

The State Route 28 project corridor, which is located between downtown Milford and Interstate 275, is an important link for the movement of traffic between Interstate 275 and the city of Milford. Typical of older urban interchanges, the buildup of commercial and residential uses in the immediate vicinity has increased traffic volumes that have congested access to properties along the existing roadway and reduced efficient access to and from the Interstate.

Purpose

The purpose of the project is to:

- Provide adequate roadway capacity and improve access management to accommodate traffic ingress and egress through the corridor.
- Provide an acceptable level of service (“C” or above) at signalized intersections along State Route 28.
- Meet current ODOT Design Standards for projected Design Year (2031) traffic volumes.

Project Background

Designated as a State Route in 1926, State Route 28 stretches 70 miles, from Milford to Chillicothe, Ohio and serves as the primary east-west arterial in northern Clermont County. Improvements to the State Route 28 corridor have been considered at various times over the last several decades as development in the Cincinnati metropolitan region has spread and Clermont County experienced rapid growth. To the west of the project limits, State route 28 and Business 28 serve as a Main Street for the city of Milford, traversing its retail core. In 2007, the Ohio Department of Transportation (ODOT) completed a project that widened State Route 28 to four lanes, from Bypass 28 to Branch Hill-Guinea Pike.

The Clermont County Transportation Improvement District (CCTID) was established in June 2006 by the Board of Clermont County Commissioners. The CCTID Board of Trustees established a regional transportation improvement program consisting of corridor level projects and supporting program-level implementation strategies and activities to put into practice a regional approach to transportation improvements in support of economic development in Clermont County. The CCTID currently has five capital improvement projects planned for the State Route 28 corridor to address access management and joint economic development. The proposed CLE-28-2.05 project is included as part of this initiative.

(See page 2-A for a continuation of this section)

ALTERNATIVES:

The proposed project involves improvements to approximately 1900 feet of State Route 28, beginning west of the Castleberry Court intersection at the western terminus and ending at the I-275 ramp terminals at the eastern terminus. This corridor is zoned for business and office districts adjacent to State Route 28, and commercial development is continuous on both sides of the project with exception of one residential parcel on the west end.

The purpose of the project can be achieved by implementing the following actions:

- Provide adequate roadway capacity and improve access management to accommodate traffic ingress and egress through the corridor.
- Provide an acceptable level of service (“C” or above) at signalized intersections along State Route 28 for projected Design Year (2031) traffic volumes.
- Meet current ODOT Design Standards, where possible, for the project design year (2031) traffic volumes..

(See page 2-D, 2-E, and 2-F for a continuation of this section)

The Do Nothing Alternative is not feasible, prudent or practicable (Mark all that apply):

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems, or

It would result in serious impacts to the motoring public and general welfare of the economy.

Yes No

X	
X	
	X
	X
X	

This is page 2 of 15, which is part of: Categorical Exclusion Level 2 Date: 11/13/2009

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Purpose & Need (continued from page 2)

Prior Plans and Studies.

ODOT District 8 Safety Study. A corridor safety study, named CLE-28 0.83-4.91, dated August 2007 was completed by ODOT District 8 for the State Route 28 corridor from Lila Avenue (MP 0.83) to Woodville Pike (MP 4.91). The study identified a total of 43 crashes at the McClelland Road and State Route 28 intersection of which 12 were rear-end crashes in the westbound approach and eight crashes on the eastbound approach between 2004 and 2006. (Rear-end crashes are indicative of congested areas, supporting the need to provide adequate capacity at this intersection.) The focus of the study involved addressing safety issues at the I-275 interchange. The proposed countermeasure was to construct a directional loop ramp in the northwest quadrant of the interchange that would allow westbound State Route 28 traffic to access southbound I-275. The westbound left turn movement at the I-275 southbound ramps would be converted to a right turn movement with this improvement thus improving intersection capacity and corridor operations.

Interchange Modification Study. The Ohio Department of Transportation (ODOT) completed an Interchange Modification Study (IMS) for the I-275/State Route 28 interchange documenting the improvements to the interchange as outlined in the previous safety study. The IMS was approved by the Federal Highway Administration (FHWA) in December 2008. Intent of the study was to design modifications to the Interstate 275 interchange that will enhance safety and improve operations of State Route 28 without degrading operations along Interstate 275. The study was developed with the understanding that the Clermont County Engineer was designing a project (CLE-28-2.05) to provide a five lane section on State Route 28 south of the interchange. The summary of recommendations includes the intent to operate the McClelland Road signal and the southbound Interstate 275 ramp intersection signal on a single controller. The single controller will ensure the maximum level of coordination between these two intersections to facilitate traffic flow.

City of Milford Access Management Plan. The City of Milford developed an access management plan in August 2000 to use as a future guide to correct current roadway related deficiencies. Two primary access improvements are proposed with the CLE-28-2.05 project:

- Consolidation of access points on the south side of State Route 28 and relocation to minimize the number of offset driveways.
- The existing raised island that is utilized to enforce the right-in/right-out (RIRO) movement at the Thornton's (STA 59+60L) will be removed from the private driveway. The redevelopment of the gas station site consolidated two existing parcels. A new raised island in the center of State Route 28 is proposed as a more effective method to restrict left turn movements from the private driveway.

Level of Service (LOS) Definition.

The following discussions of project purpose and need reference Level of Service (LOS). To aid in understanding this measurement of driver delay, the definition of LOS is provided below.

The Highway Capacity Manual 2000 manual defines the LOS as a function of the delay encountered by motorist, which is a measure of driver discomfort, frustration, fuel consumption, and lost travel time. Levels of service are given in terms of average control delay per vehicle. Control delay for signalized intersections includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. This criterion is applied to individual signalized movements and the entire intersection in the same way. The LOS criteria and characteristics are described in **Table 1**.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Table 1: Signalized Intersection Level of Service Descriptions

Level of Service	Control Delay (seconds per vehicle)	Characteristics
A	≤10	Very low delay. Occurs when progression is extremely favorable and most vehicles arrive during the green phase. Most vehicles are not required to stop.
B	> 10 and ≤ 20	Occurs with good progression and/or short cycle lengths. More vehicles are required to stop compared to LOS A.
C	> 20 and ≤ 35	Higher delays result from fair progression and/or longer cycle lengths. Individual cycle failures may begin to appear in this category. A significant number of vehicles may be required to stop, although many still pass through the intersection during the green cycle.
D	> 35 and ≤ 55	Longer delays may result from unfavorable progression, long cycle lengths, and/or high volume to capacity (v/c) ratios. Many vehicles are required to stop and the proportion and the proportion of vehicles not required to stop declines compared to the above three LOS designations.
E	> 55 and ≤ 80	Considered to be the limit of acceptable delay, these high delay values generally indicate poor progression, long cycle lengths, and high v/c ratios. Individual cycle failures are frequent occurrences.
F	> 80	Considered to be unacceptable to most drivers, this condition often occurs with oversaturation. It may also occur at high v/c ratios below 1.0 with multiple individual cycle failures.

LOS uses a letter scale from A to F as a measure of available capacity of the roadway and existing demand. LOS A indicates that a driver may maneuver on the roadway unimpeded by other drivers and with little delay. LOS E indicates the uppermost operational limit of traffic, indicating that traffic is moving with little or no maneuverability, increased delay, and lower speeds. LOS F indicates that the roadway is over capacity and is indicative of roadways that experience severe congestion and stopped or slow conditions.

Project Need.

Congestion. A capacity analysis was performed for the proposed project by LJB Inc. in October 2008. The analysis included the study of the existing lane configuration at signalized and unsignalized intersections based upon AM and PM design hour volumes for year 2031.

McClelland Road and State Route 28 form a three-legged intersection 250 feet west of the I-275 southbound ramp terminals. Congestion extends through the McClelland Road intersection which has only 250 feet of spacing to the I-275 interchange ramps. Two of the three existing intersection legs experience excessive approach vehicle delays resulting in poor LOS and backups where vehicles often fail to clear the intersection efficiently. During the Design Year PM peak hour, average vehicle delay is projected to be 64.4 seconds and 55.5 seconds for southbound McClelland Road and westbound State Route 28, respectively. This delay will result in LOS of E for each of these approaches. The two westbound through lanes on State Route 28 are reduced to a single through lane at the McClelland Road intersection, contributing to the congestion on the corridor.

As part of the capacity analysis a storage lane analysis was conducted. Because the McClelland Road intersection is only 250 feet west of the southbound ramp terminal intersection, only 11 to 12 cars can be stored between these signalized intersections without blocking the I-275 ramp terminals. Peak hour in 2031, requires 41 westbound cars to clear this intersection every signal cycle. With limited distance between intersections, westbound backups resulting

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

from the deficient approach LOS E can quickly spread east across the I-275 interchange creating “gridlock” at the signalized ramp terminals. Once this occurs, recovery is not expected until peak hour volumes eventually dissipate.

At the Castleberry Court/State Route 28 intersection, the 2031 No Build projected peak hour LOS is D for all four legs and the intersection.

Table 2: Capacity Analysis Summary at Signalized Intersections

Location	2031 AM Existing Laning	2031 PM Existing Laning
SR 28 at...	Intersection LOS/ Delay (seconds)	Intersection LOS/ Delay (seconds)
Castleberry Court	B / 16.9	D / 35.5
McClelland Road	C / 25.9	D / 37.8

Access Management.

The corridor contains several commercial developments with opposing driveways that are offset from the drives on the opposite side of the road. ODOT’s Access Control Guidelines document that resulting erratic vehicle movements resulting from misaligned driveways, reduces roadway capacity and increases accident potential.

As identified in the City of Milford Access Management Plan, there is a need to correct current roadway related deficiencies through consolidation and relocation of access points to minimize the number of offset driveways.

Pedestrian Access.

Sidewalks are currently located at the western end of the project at the new U.S. post office along Castleberry Court as well as along the residential section of McClelland Road, which is located at the eastern end of the project. Various commercial businesses are situated in between these two project limits. The proposed sidewalks will serve to provide a much higher level of pedestrian connectivity between the residential areas and the commercial areas within the project corridor as well as improving safety for those walking along State Route 28.

Logical Termini.

The limits of the logical termini have primarily been identified through capacity analysis of the roadway. The capacity analysis, conducted by LJB Inc. in October 2008, determined that extending a lane west of Castleberry Court was not needed. State Route 28 west of Castleberry Court experiences a slightly lower traffic volume and fewer driveways. Therefore the Castleberry Court intersection was identified as the western terminus of the project. The eastern terminus of the project meets up with the I-275 interchange with State Route 28. The capacity analysis determined that this eastern limit adequately addresses the capacity needs of State Route 28 through this corridor. Also, ODOT is developing plans to modify the interchange itself (and coordinating design development with this State Route 28 project). Therefore State Route 28 immediately west of the I-275 southbound entrance and exit ramps was identified as the eastern terminus of the project.

Independent Utility:

The project location was identified within both ODOT’s IMS and the City of Milford’s Access Management Plan as an area for which roadway improvements were warranted. However, although this section of State Route 28 was analyzed under two separate studies as a part of a greater improvement plan, it was determined by ODOT under the 2007 safety study that the proposed work under this project could be installed independently without adversely impacting the abutting sections of State Route 28. Thus, the purpose and need of the project could be met without the need for subsequent adjacent roadway improvements. Therefore, the specific project upgrades were independent of any other transportation project within the area.

This is page 2-C of 15, which is part of: Categorical Exclusion Level 2 Date: 11/13/2009

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

The project is not divided into interdependent phases, will be constructed under one construction contract, stands alone as an independent project. and therefore, establishes independent utility.

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Alternatives (continued from page 2)

The alternative solutions that will achieve the purpose of the project are limited by the constraints of the roadway character, and the relatively short distance of 1900 feet between the Castleberry Court intersection to the west and the I-275 interchange ramp terminals immediately to the east of the project.

The alternatives of shifting the alignment (and resulting impacts) exclusively to either the north side or the south side of State Route 28 were initially considered in addition to widening symmetrically on each side following the existing alignment. Shifting the widening to one side or the other was dismissed for the following reasons:

1. The I-275 ramp terminals and Castleberry intersections represent “fixed” points where the State Route 28 lanes must line up for smooth transition. With the relative short project length, neither roadway tapers nor reverse curves could provide an acceptable shift either to the north or south. As an example, using tapers to shift for a 12-foot lane addition would require 960 foot long transition. Additional width needed for sidewalks, median and grading would further lengthen the transition areas. With a total project length of 1860 feet, shifting to one side or the other is neither practical nor desirable. In addition, at the transition areas at each end, the roadway can not be shifted fast enough to avoid some work on both sides of the road.
2. Introducing curves or transitions in a short straight section of roadway reduces efficient traffic flow.
3. If the road is widened symmetrically, the impacts to adjacent properties are minimal. If the widening is offset to one side, impacts to properties on that side become considerably greater. These impacts would affect parking, commercial drives, and internal parking lot circulation of businesses on the side of the shift.
4. Widening symmetrically salvages the majority of the existing State Route 28 pavement and roadway. Shifting to one side would require total reconstruction of existing State Route 28 to accommodate the revised alignment and meet pavement cross section standards.

Salvaging as much of existing State Route 28 as possible by widening symmetrically was selected for the Preferred Alternative.

Alternative 1: No-Build

The No Build Alternative does not make any major improvement to State Route 28 within the project area. In this alternative the roadway would continue to be maintained as it exists today and therefore would not address roadway congestion and access management issues through this section of State Route 28.

Capacity (See Table 3 below). Under this alternative, congestion extends through the McClelland Road intersection which is located about 250 feet from the I-275 interchange. Two of the three intersection legs at the McClelland Road intersection experience excessive approach vehicle delays resulting in poor levels of service (LOS) and backups where vehicles can fail to clear the intersection efficiently. During the Design Year 2031 of the PM peak hour for the No-build condition, average vehicle delay is projected to be 64.4 seconds and 61.7 seconds for southbound McClelland Road and westbound State Route 28, respectively. This results in LOS E for each of these approaches. Note that two westbound through lanes on State Route 28 are reduced to a single through lane at the McClelland Road intersection contributing to the congestion to the corridor in the No-build condition.

Because the McClelland Road intersection is only 250 feet west of the southbound interstate ramp terminal intersection, only approximately 12 cars can be stored between these signalized intersections without blocking the I-

This is page 2-D of 15, which is part of: Categorical Exclusion Level 2 Date: 11/13/2009

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

275 ramp intersection. Peak hour 2031 volumes require 41 westbound cars to clear this intersection every signal cycle. With limited distance between intersections, westbound backups (resulting from deficient roadway capacity west of the intersection), approach LOS E can quickly extend east across the I-275 interchange, blocking the signalized ramp intersection. When this scenario occurs, recovery is not expected until peak hour volumes dissipate.

Table 3 below shows the results of the capacity analysis for the Castleberry Court and McClelland Road intersections with State Route 28. By 2031 the analysis forecasts the No-build PM peak hour to have a LOS D at Castleberry Court, with 35.4 seconds of delay. The No-Build 2031 PM peak hour prediction for McClelland Road is a LOS D with 37.8 seconds of delay.

Access Management. Under the No-build alternative, there will be no consolidation or relocation of the numerous commercial driveways that are offset from the drives on the opposite side of the road. As identified in the City of Milford Access Management Plan, there is a need to correct current roadway related deficiencies through consolidation and relocation of access points to minimize the number of offset driveways.

No-Build Conclusions. As described above, the No-build alternative will not provide an acceptable level of service ("C" or above) at signalized intersections along State Route 28. Access management will not be improved to accommodate traffic ingress and egress through the corridor.

For these reasons, Alternative 1 (No-build) does not meet the purpose and need for this project and is not considered a feasible alternative.

Table 3: Capacity Analysis Summary of Signalized Intersections

Location	2031 AM No-build	2031 PM No-build	2011 AM Preferred Alternative	2011 PM Preferred Alternative	2031 AM Preferred Alternative	2031 PM Preferred Alternative
SR 28 at...	LOS/ Delay (seconds)	LOS/ Delay (seconds)	LOS/ Delay (seconds)	LOS/ Delay (seconds)	LOS/ Delay (seconds)	LOS/ Delay (seconds)
Castleberry Court intersection	B / 16.9	D / 35.4	B / 16.4	C / 33.9	B / 16.2	C / 34.1
EB Left	B / 14.3	C / 34.6	B / 12.4	D / 36.5	B / 13.2	C / 34.6
EB Thru-Right	B / 10.1	D / 35.2	A / 8.8	D / 35.0	A / 8.3	D / 35.2
WB Left	A / 0.8	A / 2.9	A / 0.8	A / 5.9	A / 0.8	A / 9.5
WB Thru	C / 20.4	D / 35.3	C / 22.1	C / 34.1	C / 21.8	C / 33.5
WB Right			A / 0.9	A / 1.6	A / 0.8	A / 4.4
NB Left-Thru-Right	C / 28.8	D / 39.3	C / 29.5	D / 35.6	C / 31.3	D / 39.3
SB Left-Thru	C / 29.1	D / 40.6	C / 29.8	D / 36.7	C / 31.6	D / 40.6
SB Right	C / 29.0	D / 41.4	C / 29.7	D / 37.5	C / 31.5	D / 41.4
McClelland Road intersection	C / 25.9	D / 37.8	B / 20.0	C / 22.2	C / 20.3	C / 23.4
EB Left	C / 28.0	D / 50.6	B / 16.6	C / 28.0	B / 17.9	D / 44.0
EB Thru	A / 3.7	B / 11.3	C / 20.1	B / 11.1	B / 20.0	B / 11.4
WB Thru	D / 39.5	E / 61.7	B / 19.6	C / 29.2	C / 20.2	C / 30.9
WB Right	A / 1.9	B / 17.4				
SB Left-Right	D / 49.9	E / 64.4	C / 22.7	C / 35.0	C / 23.3	C / 35.0

Alternative 2: Preferred Alternative

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Alternative 2 would create a uniform five lane section, widened symmetrically, by adding turn lanes from Castleberry Court to the I-275 ramp terminals. Curb and gutter with a 5.5 foot sidewalk will be added on each side. Two-way left-turn lanes or exclusive left turn lanes will be provided throughout the project length. No additional through travel lanes are created by this project. Please see the Project Description section above and page 19 of Appendix A for additional details.

Capacity (See Table 3 above). Alternative 2, labeled as “Preferred Alternative” in Table 3, improves the LOS and reduces average vehicle delays at the McClelland Road and Castleberry Court intersections. Assessing data for the 2031 PM peak hour, the westbound through and southbound movements at McClelland Road show nearly 50% reductions in the average vehicle delay, reflecting improvements from LOS E for the No-build to LOS C for the Preferred Alternative. The averaged level of service and vehicle delay for the overall intersection at McClelland Road and State Route 28 improves from LOS D and 37.8 second delay for No-build, to LOS C and 23.4 second delay for the Preferred Alternative. Current westbound vehicle backups often extend beyond the southbound I-275 ramp terminals, creating a “gridlock” situation at the ramp terminal. Reducing the westbound traffic queue from the McClelland Road signal will improve the interchange operation and eliminate the potential for “gridlock” at the southbound off-ramp.

The Preferred Alternative also improves operations at the intersection of Castleberry Court and State Route 28. In design year 2031, the level of service is expected to be improved from LOS D under the No-build Alternative to LOS C with the Preferred Alternative.

Access Management. Alternative 2 will improve access management through the construction of a raised median east of Old Bank Road (to control left-turn movements). The project will further improve access management along the corridor by removing or relocating multiple drives to eliminate offsets and improve intersection geometry. These improvements are based upon the access management plan developed by the city of Milford in August 2000.

Preferred Alternative Conclusions. The Preferred Alternative will improve the LOS and reduce delays. The improvement will provide an acceptable level of service (“C” or above) for projected Design Year (2031) traffic volumes and meet current ODOT Design Standards for this type of roadway facility. The Preferred Alternative meets the purpose and need of the project.

Appendix E (pages 100 to 129) provides selected plan sheets from the stage of design of the preferred alternative.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

ROADWAY CHARACTER:

Functional Classification: Minor Urban Arterial
 Current ADT: 28640 vpd 20(08) Design Year ADT: 30950 vpd (20 31)
 DHV: 3095 Trucks, .01 %
 Designed Speed: 40 mph Legal Speed: 35 mph

Existing **Proposed**

Number of Lanes:	<u>3 to 5</u>	<u>5 to 6</u>	
Type of Lanes:	<u>2-4 Through and 1-turn</u>	<u>2-4 Through and 1-2 turn</u>	Includes turning lanes
Pavement Width:	<u>34 to 55</u> ft.	<u>60</u> ft.	
Shoulder Width:	<u>2 to 8</u> ft.	<u>2</u> ft.	
Median Width:	<u>N/A</u> ft.	<u>Varies 0 to 8</u> ft.	
Sidewalk Width:	<u>None</u> ft.	<u>5.5</u> ft.	

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

DESIGN CRITERIA FOR BRIDGES:

Structure File Number(s): No structures located in the corridor Sufficiency Rating: N/A

Existing **Proposed**

Bridge Type:	<u>N/A</u>		
Number of Spans:	_____	_____	
Weight Restrictions:	_____ ton	_____ ton	
Height Restrictions:	_____ ft.	_____ ft.	
Curb to Curb Width:	_____ ft.	_____ ft.	
Shoulder Width:	_____ ft.	_____ ft.	
Under Clearance:	_____ ft.	_____ ft.	

Will the structure be rehabilitated or replaced as part of the project?
 If Yes, has an asbestos inspection been completed?

Y	N
<input type="checkbox"/>	<input type="checkbox"/>

DESIGN CRITERIA FOR BRIDGES:

Structure File Number(s): _____ Sufficiency Rating: _____

Existing **Proposed**

Bridge Type:			
Number of Spans:	_____	_____	
Weight Restrictions:	_____ ton	_____ ton	
Height Restrictions:	_____ ft.	_____ ft.	
Curb to Curb Width:	_____ ft.	_____ ft.	
Shoulder Width:	_____ ft.	_____ ft.	
Under Clearance:	_____ ft.	_____ ft.	

Will the structure be rehabilitated or replaced as part of the project?
 If Yes, has an asbestos inspection been completed?

Y	N
<input type="checkbox"/>	<input type="checkbox"/>

This is page 3 of 15, which is part of: Categorical Exclusion Level 2 Date: 11/13/2009

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

MAINTENANCE OF TRAFFIC DURING CONSTRUCTION:

	Y	N
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: The proposed maintenance of traffic is a five-phased part width construction and will require a minimum of one lane of traffic be maintained at all times during construction. The sequence of construction is as follows:

The Pre-construction Phase will construct the proposed drainage system and associated grading. This phase will also involve milling and repaving of the existing pavement between Sta. 47+00 and Sta. 52+00.

Phase 1 will involve the placing of temporary pavement markings, constructing the pavement widening along the north side of State Route 28 from Station 46+20 to Interchange 275, and planing existing pavement where needed to maintain a flush condition between existing pavement and newly constructed pavement. At the Castleberry Court intersection (west) the return radii will be constructed, the signal revised, and full depth pavement constructed. At the McClelland Road intersection, this phase will construct the return radii and resurface and widen the intersection. At Milford Hills Drive, pavement buildup and widening will occur.

See page 4-A for a continuation of this section

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ _____ Right-of-Way: \$ 500,000 Construction: \$ 2,500,000
 Anticipated Start Date of Construction: May, 2011

RIGHT OF WAY AND UTILITY INVOLVEMENT:

Number of parcels to be affected for temporary ROW: 15
 Number of parcels to be affected for permanent ROW: 15
 Approximate area of temporary right-of-way needed: 1.8 acre
 Approximate area of permanent right-of-way needed: 1.5 acre

Has Utility Coordination been completed?	Yes	<input checked="" type="checkbox"/>	No	<input type="checkbox"/>
Are large scale transmission facilities located within the project area?	Yes	<input type="checkbox"/>	No	<input checked="" type="checkbox"/>
Are there any private utility easements within the project area?	Yes	<input checked="" type="checkbox"/>	No	<input type="checkbox"/>
If YES, will it be impacted by the project?	Yes	<input checked="" type="checkbox"/>	No	<input type="checkbox"/>

Remarks: There are 15 total parcels affected by this project, all having both temporary and permanent takes. The approximate proposed "worst-case" right-of-way includes 1.5 acre of permanent (including both warranty deed and storm sewer easement) and 1.8 acre of temporary right-of-way for the purpose of grading. Where permanent right of way is required, the approximate average width of the permanent acquisition is 23 feet.

Utilities affected by the proposed project include: water, sanitary, telecommunications, gas, and electric. The project sponsor and design consultant sent initial contact letters to all utilities on August 12, 2008 and held a Stage 1 design utility coordination meeting on April 2, 2009. Coordination with utilities has continued through Stage 2 design. All utility coordination required during construction will be detailed as notes in the plans.

See page 4-A for a continuation of this section

Form version: 11/16/044

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Maintenance of Traffic During Construction (continued from page 4)

Phase 1A – This phase will plain the remaining existing pavement and place intermediate course on the newly constructed pavement and existing pavement.

Phase 2 – During this phase the widening pavement along the south side of State Route 28 will be constructed to the top of the intermediate course. The Castleberry Court intersection return radii will be built, along with full depth pavement to the top of the intermediate course. At Old Bank Road, construction of the return radii will be performed with the resurfacing and widening of intersection.

Phase 3 - Pavement markings will be revised, the raised median from Sta. 56+92.00 to Sta. 58+18.00 will be constructed and surface course will be placed.

Access will be maintained to all businesses at all times. There are no existing sidewalks along State Route 28, so construction will not impact any existing pedestrian facilities. Upon the completion of Phase 1, proposed sidewalks will be available along the north side of State Route 28 for the remainder of construction. Likewise, proposed sidewalks along the south side of State Route 28 will be available to pedestrians upon the completion of Phase 3. A total of five Phases will be required in order to maintain access to all parcels throughout the duration of the project. Due to the frequency of driveway entrances and side road intersections to maintain, drums are recommended as the channelizing device.

Construction Notification - The Contractor will advise the Project Engineer a minimum fourteen (14) days prior to the start of construction activities and/or any lane closures or detours. The Project Engineer will forward this information to the ODOT District 8 Public Information Officer (PIO) by fax at (513) 932-7651 or at the ODOT District 8 PIO notification website, <http://www.dot.state.oh.us/districts/D08/pages/PlanningPIOWebform.aspx>. The PIO will then provide written notification to the local emergency responders, schools, and businesses of any upcoming lane closures and/or detours.

Right of Way and Utility Involvement (continued from page 4)

The widening of the roadway, including the addition of sidewalks, will require the relocation and/or adjustment of fire hydrants, water main valves, and valve and service boxes. A water meter pit, located at 46+00 LT, is located under the proposed sidewalk and curb ramp, and will remain and become integral with the new sidewalk and curb ramp.

Gas main valve boxes will be adjusted to proposed grades throughout the project. Duke Energy will relocate their 6-inch gas main under the new sidewalk prior to construction of this project. Gas lines and meters servicing individual adjacent properties will remain outside of the public right of way. All power poles and electric boxes that are within the proposed pavement and sidewalk areas will be relocated at a to provide minimum recommended clear zone requirements of 1.5 feet for a low speed urban street. The extent of effect to the sanitary sewer system is expected to be the reconstruction of existing manholes to grade. Time Warner Cable is located within the corridor and will also require adjustment.

The project sponsor will be responsible for coordination with utility owners for utility relocations that need to occur prior to construction of the project. These utilities include Duke Energy (gas), Cincinnati Bell and Time Warner Cable (telecommunications). Below grade, Duke's gas main and Cincinnati Bell's fiber optic line require relocation prior to construction. Above grade, poles owned by Duke and Cincinnati Bell require relocation. The above grade relocation will also affect Time Warner Cable.

This is page 4-A of 15, which is part of: Categorical Exclusion Level 2 Date: 11/13/2009

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Part II – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

	Presence		Impacts	
	Y	N*	Y***	N**
Streams, Rivers & Watercourses	X			
National Scenic River		X		
State Wild, Scenic or Recreational River		X		
Commercial				
Non-Commercial				
OEPA Aquatic Life Use Designation (eg. WWH)				

Ephemeral Headwater Stream

Remarks: The MOA Level Ecological Survey Report (ESR) prepared by LJB Inc. on November 25, 2008. See pages 17 and 18 of Appendix A for the location of area streams on the USGS quadrangle topographic map and aerial.

One jurisdictional stream (Stream 1) is located within the study area. Stream 1 is an ephemeral headwater stream with a muck dominated substrate. The stream begins abruptly at STA 52+00L on the north side of State Route 28, where an 18 inch pipe outlets from under the roadway. South of the roadway, the area is entirely paved and there are no streams evident. This stream within the study area flows northward and ultimately into the Little Miami River, which is located approximately 0.8 aerial miles and 1.4 stream miles from the study area. The Little Miami River is a State and National Scenic River and is an OEPA designated Exceptional Warm Water Habitat.

It is anticipated that a maximum of 60 linear feet of impact will occur in this stream. In stream work includes approximately 42 foot extension of the 18 inch culvert and 20 foot for installation of a headwall and rock channel protection. The proposed project will impact the stream within the study area during construction by increased siltation and direct aquatic habitat destruction. Based on the nature of flowing water, the siltation effects should only be temporary. Although the proposed roadway improvements are within 0.8 of a mile of the Little Miami River, no impacts are proposed within the Little Miami River itself.

The Agency Coordination section below provides a summary of project coordination with ODNR and USFWS. Coordination letters are provided on pages 32 to 55 in Appendix B.

	Presence		Impacts	
	Y	N*	Y***	N**
Other Surface Waters		X		
Reservoirs				
Lakes				
Farm Ponds				
Detention Basins				
Storm Water Management Facilities				
Other: _____				

Remarks: LJB Inc. conducted on-site evaluations during the site visits on April 16, 2008, May 19, 2008 and October 12, 2008 for the preparation of the MOA-Level Ecological Survey Report. These site visits and a review of aerial and USGS mapping, LJB concluded no reservoirs, lakes, farm ponds, detention basins, or storm water management facilities are located in the project area.

A photo log of the project area including an aerial photograph is included in Appendix D starting on page 90.

*If the resource is not present, the remainder of this subject section will not be completed
 **If the resource is present but no impacts are anticipated, the reason why is described under Remarks.
 ***Any impacts, mitigation, and agency coordination are described under Remarks and coordination letters are attached.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

<u>Presence</u>		<u>Impacts</u>	
Y	N****	Y***	N**
	X		

Wetlands

Total wetland area impacted: _____ acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Non-isolated Wetland	Isolated Wetland
OEPA Wetland Category: _____	OEPA Wetland Category: _____
Size of Area Impacted: _____ acre(s)	Size of Area Impacted: _____ acre(s)

Wetlands	<u>None Present</u>	Documentation	
		Y	N
Wetland Determination			
Wetland Delineation Report			
Individual Wetland Finding			
Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):			
Substantial adverse impacts to adjacent homes, business or other improved properties;			
Substantially increased project costs;			
Unique engineering, traffic, maintenance, or safety problems;			
Substantial adverse social, economic, or environmental impacts, or			
The project not meeting the identified needs.			
USACOE Isolated Waters Determination			
Mitigation Plan			
<i>Measures to avoid, minimize and mitigate wetland impacts need to be discussed in the remarks section</i>			

Remarks: The MOA Level Ecological Survey Report (ESR) prepared by LJB Inc. on November 25, 2008. The on-site evaluation of wetlands was conducted during the site visit on April 16, 2008. LJB found no evidence of wetlands in the study area.

The National Wetland Map did not indicate any wetlands within the study area.

<u>Presence</u>		<u>Impacts</u>	
Y	N****	Y***	N**
X			
	X		

Terrestrial Habitat
Unique or High Quality

Remarks: LJB Inc. prepared a MOA Level Ecological Survey report for the project on November 25, 2008. LJB conducted an on-site evaluation of habitat during the site visit on April 16, 2008 and observed that proposed project-affected vegetation consists of mowed roadside vegetation, predominantly grass, within the existing right-of-way adjacent to the existing pavement. Based on this information, the affected terrestrial habitat is not considered to be unique or high quality. Photographs showing the project area are provided in Appendix D starting on page 90.

** If the resource is present but no impacts are anticipated, describe the reason why in the Remarks section.
 ***Any impacts, mitigation and agency coordination are described under Remarks and coordination letters are attached.
 ****If "no", discuss in the Remarks details how this determination was made.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Presence

Impacts

Threatened or Endangered Species

Within the known range of and federal species?
 Federal species found in project area?
 State species found in project area?
 Is the project in accordance with the Letter of
 Agreement on Endangered Species Coordination?

	Y	N****		Y***	N
	X				X
		X			
		X			
	X				

Remarks:

The project is within the known range of the Indiana Bat (*Myotis sodalists*, E), the Rayed Bean Mussel (*Villosa fabalis*, C), the Sheepnose Mussel (*Plethobasus cyphus*, C), and Running Buffalo Clover (*Trifolium stoloniferum*, E). LJB Inc. reviewed Natural Heritage maps and conducted a field review on May 19, 2008 and found no trees exhibiting suitable Indiana Bat roosting habitat that will be removed for this project. Also noted was that Stream 1 does not offer suitable mussel habitat. Upon review of LJB's MOA-Level ESR, ODOT-OES determined that, based on the project type, location, and on-site habitat, none of these species except the Running Buffalo Clover would be expected within the project area, and no impacts to these species are expected.

Running Buffalo Clover – One small area near Stream 1 in the central portion of the study area offers potential suitable habitat for this species. Biologists from LJB Inc. surveyed this parcel on May 19, 2008 for this species, and no individuals were found. Based on the above discussion, ODOT-OES determined that this project may affect, but is not likely to adversely affect this species. On December 24, 2008, the U. S. Fish & Wildlife Service comments concurred in this determination for Running Buffalo Clover (*Trifolium stoloniferum*, E). ODNR had no comments on endangered species for this project.

The Agency Coordination section below provides a summary of project coordination with ODNR and USFWS. Coordination letters are provided on pages 32 to 55 in Appendix B.

Coordination

Approval

Agency Coordination ***

Ohio Department of Natural Resources (ODNR)
 United States Fish and Wildlife Service (USFWS)
 Ohio Environmental Protection Agency (OEPA)
 United States Army Corps of Engineers (USACE)
 ODNR State Scenic River
 National Park Service (NPS) National Scenic River

	Y	N		Y	N
	X			X	
	X			X	
		X			
		X			
	X			X	
		X			

Remarks:

On December 19, 2008, ODOT Office of Environmental Services sent project notification letters, including the MOA-Level ESR to the Ohio Department of Natural Resources and the U.S. Fish & Wildlife Service for concurrence and/or comments. These letters and responses from ODNR and USFWS are on pages 32 to 55 in Appendix B.

On January 29, 2009, ODNR, Division of Natural Areas and Preserves provided comments on this project. They stated "this project is within the Little Miami State and National Scenic River watershed, and within one mile of the Little Miami." ODNR's recommendations and their current dispositions are summarized as follows:

1. A sediment and erosion control plan should be developed for the site and implemented before earthwork commences. Particular attention should be given to any drainage ways, ditches and streams that could convey sediment laden water directly to the unnamed tributary of the Little Miami. Properly installed (framed and entrenched) sediment fence should be utilized around the work site perimeter and storm water inlets. Appropriately designed rock-check dams and other erosion controls should be utilized in ditches and drainage ways. All controls should be properly maintained until final site stabilization is achieved. All sediment and erosion controls should be removed upon stabilization of the project area with vegetation. Straw bales should not be permitted as a form of erosion control.

See page 7-A for a continuation of this section)

*If the resource is not present, the remainder of this section will not be completed.
 **If the resource is present but no impacts are anticipated, the reason why is described under Remarks.
 ***Any impacts, mitigation, and agency coordination are described under Remarks and coordination letters are attached.
 ****If "no", discuss in the Remarks details how this determination was made.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Agency Coordination (continued from page 7)

All denuded areas, including ditches, culverts and river/stream banks, should be permanently seeded and mulched (or fiber mat) immediately upon completion of earthwork or temporarily seeded and mulched (or fiber mat) within seven days if the area is to remain idle for more than thirty days. Access roads constructed on slopes shall be graveled to prevent erosion from surface runoff. *Disposition: The above recommendations will be addressed by applying "Supplemental Specification 832-Temporary Sediment and Erosion Control" to the project plans. A plan note will also be included that will not permit the contract to use straw bales as a form of erosion control.*

2. Idle equipment, petrochemicals and toxic/hazardous materials should not be stored ...near any drainage ways, ditches or streams that could convey such materials to the unnamed tributary of the Little Miami. Petrochemicals and toxic/hazardous materials should not be discharged into the unnamed tributary of the Little Miami ...or any drainage ways, ditches or streams. Refueling of equipment shall not occur ...near any drainage ways, ditches or streams. A spill containment and cleanup plan should be generated prior to the start of the project. *The above recommendations will be addressed by applying "Supplemental Specification 832-Temporary Sediment and Erosion Control" to the project plans.*

3. All in-stream or stream crossing work should be conducted during low flow period (August 1 through October 31). Any disturbed areas in the stream beds should be returned to pre-construction contours. Stream bottom elevations should be determined before in-stream work commences to ensure that all fill material and debris is completely removed before construction is completed. Rip-rap used should be kept to the minimum amount needed to prevent scour and shall consist of clean rock only (free of any toxic or fine material). All fill material used as rip-rap, work platforms or cofferdams should be a minimum of three inches in diameter and be washed to remove fine particulate matter (clay, silt, sand and soil). ...In-stream work should be conducted through the use of water diversions not requiring the placement of earthen fill (sheet piling, membrane dams, etc.) wherever possible. Any fill should be completely removed from the streambed immediately upon completion of in-stream work. *The stream crossing in question begins abruptly at STA 52+00L on the north side of State Route 28, where an 18 inch pipe outlets from under the roadway. South of the roadway, the area is entirely paved and there are no streams evident. The source of this stream, at this point of its headwaters, originates from storm sewer runoff of the paved commercial areas south of State Route 28. The stream flows only for a short time after rainfall. Therefore low flow periods are frequent throughout the construction season. Considering this and that the proposed stream work can be performed in approximately two days, a plan note will be developed that will allow the Contractor to begin the proposed in-stream work outside the low-flow dates only when the stream is dry and there is no forecasted precipitation for the next 48 hours. Work shall be completed within this time frame. Schedule of this work shall be approved by the Project Engineer prior to commencement.*

4. If dewatering becomes necessary throughout any portion of the project, all wastewater should be pumped onto a vegetated area a sufficient distance from the unnamed tributary of the Little Miami to allow for complete infiltration. No wastewater of any kind should be discharged directly into the unnamed tributary of the Little Miami or any other drainage ways, ditches or streams. All storm water drainage should be directed onto a vegetated area to allow for complete infiltration. If discharge to a vegetated area is not feasible, then wastewater should be discharged into a sediment filter bag or into a temporary detention/retention pond with sufficient retention time to permit for the settling of all suspended solids. *Applicable sections of the above recommendation will be included as a plan note for dewatering.*

5. All streambank vegetation should be left undisturbed to the maximum extent possible. Areas where vegetation is removed should be re-vegetated with native tree species. Any disturbed stream banks should be returned to previously existing contours and elevations. Trees should be one inch in diameter and balled/ burlap nursery stock. After a full growing season for the trees, any stakes and guide wires should be removed and properly disposed of. Any trees that die during the first growing season should be replaced. *Applicable sections of the above recommendation will be included as a plan note for vegetation and tree replacement.*

On December 24, 2008, the U. S. Fish & Wildlife Service provided comments. They stated, "We note and appreciate your thorough description of project-specific conditions regarding potential Indiana bat roosting and maternity habitat and efforts to avoid impacts to such habitat. Project Cle-28-2.05 (PID 82140) is within the known range of the

This is page 7-A of 15, which is part of: Categorical Exclusion Level 2 Date: 11/13/2009

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Running Buffalo Clover (*Trifolium stoloniferum*), a Federally-listed Endangered Species. A survey for this species was conducted by LJB Inc. on May 19, 2008, and no individuals were found. Therefore, the Service concurs with your determination that this project, as proposed, may affect but not likely to adversely affect the running buffalo clover.”

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

SECTION B – OTHER RESOURCES

	<u>Presence</u>		<u>Impacts</u>	
	Y	N*	Y	N
Drinking Water Resources		X		
Sole Source Aquifer				
Source Water Protection Area(s)				
Public Water System(s)				
Groundwater Source				
Surface Water Source				
Residential Well(s)				

Remarks: The City of Milford provides public water service to the corridor which will not be affected by this project, except as described in the "Right of Way and Utility Involvement" on page 4-A. The water source supplying this public water service is located outside the project area.

See page 8-A for a continuation of this section

	Y	N*	Y	N
Flood Plains		X		
Longitudinal Encroachment		X		
Transverse Encroachment		X		
Is the project located in a regulated floodplain?		X		
Will the proposed project result in an encroachment in the designated floodway?		X		
Will the proposed project result in an increase in the 100-year base flood elevation discharge?		X		
Does the project conform to the local flood plain standard?		N/A		

Remarks: This project does not encroach on a Federal Emergency Management Agency (FEMA) designated floodplain, according to the FEMA Flood Insurance Rate Map for Clermont County and Incorporated Areas (Map Number FM39025C0117G, effective March 16, 2006). The project area is entirely within Zone X, defined by FEMA as other flood areas, generally slightly above the 100 year floodplain area. Because there is no encroachment on any 100-year floodplain by the proposed project, no coordination is needed with the local floodplain coordinator, and a floodplain permit will not be required for this project. See the FEMA Flood Zone Designations map on page 20 in Appendix A.

	Y	N*	Y	N
Farmland		X		
Active Agricultural Lands		X		
Agricultural District		X		
Project in compliance with ORC 929.05(a)	X			X
FPPA Project Screening Sheet	X			X
Farmland Conversion Impact Rating Sheet		X		

Remarks: LJB Inc. conducted a field visit and examined aerial photography to determine that the land use adjacent to the project corridor is fully developed, consisting primarily of retail, office, and commercial land use. On November 19, 2008, ODOT District 8 approved the findings of a Farmland Protection Policy Screening Sheet (see page 55 in Appendix B). This screening documented that since the project area is devoid of any farmland or agricultural activity Completion of the Farmland Impact Rating form (USDA Form AD-1006) is not required because the project will not affect farmland as defined in 7CFR part 658, as amended, or because the project falls within the criteria in the 1984 Memorandum of Understanding between ODOT, FHWA and USDA/SCS.

* If the resource is not present, the remaining boxes for this subject section will not be completed. State how and who made this determination.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Drinking Water Resources (continued from page 8)

LJB Inc. reviewed the ODNR well log records and found no indication of potable water wells adjacent to the project.

LJB reviewed OEPA data and confirmed that the project does not lie within the Sole Source Aquifer Boundaries of the Little Miami River, as shown on page 21 in Appendix A. LJB also found that the project does not lie within a source water protection area and no public water systems exist in the project area, as shown on page 22 in Appendix A.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

SECTION C – CULTURAL RESOURCES

	<u>Results of Research</u> Eligible and/or Listed Resource Present		<u>Project Effect</u>		
	Y	N	No Historic Properties Affected	No Adverse Effect	Adverse Effect
Prehistoric Archaeology		X			
Historic Archaeology		X			
History/Architecture		X			
NRHP Buildings/Sites		X			
NRHP Districts		X			
NRHP Bridges		X			

Documentation

		<u>SHPO / OES / FHWA Approval Dates</u>
Phase I Short Report	X	Feb 4, 2009, based upon the 7/17/06 Programmatic Agreement
Phase I Cultural Resources Survey Report		
Phase I History/Architecture Survey Report		
Phase I Archaeology Survey Report		
Phase II Cultural Resources Survey Report		
Phase II History/Architecture Survey Report		
Phase II Archaeology Survey Report		
Phase III Archaeology Data Recovery		
Documentation for Consultation / MOA		
HABS / HAER Documentation		

Remarks:

LJB submitted a cultural resource coordination package to ODOT on December 10, 2008. Subsequently, staff from ODOT's Office of Environmental Services (OES) conducted further literature searches on January 22, 2009 and completed a field review on January 23, 2009. Pages 57 to 59 in Appendix B contains a copy of the OES inter-office communication dated February 4, 2009 concluding that "no historic properties affected" is the appropriate finding for the proposed highway project based on the following:

1. No significant history/architecture resources will be effected by the undertaking.
2. The concrete box culvert is not eligible for the National Register of Historic Places.
3. No previously recorded archaeological resources are found within the APE (area of potential effect).
4. The disturbed areas across the APE precludes the existence of any significant archaeological resources.

This determination is in accordance with Stipulation 4B of the *Programmatic Agreement Among The Federal Highway Administration, The Advisory Council On Historic Preservation, The Ohio Historical Society, State Historic Preservation Office, And The State Of Ohio, Department of Transportation Regarding The Implementation Of The Federal-Aid Highway Program In Ohio (Agreement No. 12642)* executed July 17, 2006. This Programmatic Agreement is the basis for the Section 106 approval. The February 4, 2009 IOC completes the Section 106 review and no further cultural resource investigations are required.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

SECTION D – SECTION 4(F) RESOURCES

	<u>Presence</u>		<u>Impacts</u>		<u>FHWA / OES approval dates</u>
	Y	N****	Y***	N**	
Parks & Other Recreational Land		X			
Publicly owned park		X			
Publicly owned recreation area		X			
National Wild & Scenic River		X			
Section 4(f) Determination of No-Use		X			
Programmatic Section 4(f) Evaluation		X			
Individual Section 4(f)		X			
Section 6(f) involvement		X			
Other (school, state/national forest, bikeway, etc.)		X			

	<u>Presence</u>		<u>Impacts</u>		<u>FHWA / OES approval dates</u>
	Y	N****	Y***	N**	
Natural & Wildlife & Waterfowl Refuges					
Federal					
National Wildlife Refuge		X			
National Natural Landmark					
State					
State Wildlife Area		X			
State Natural Preserve					
Section 4(f) Determination of No-Use		X			
Programmatic Section 4(f)		X			
Individual Section 4(f) Evaluation		X			
Section 6(f) involvement		X			

	Y	N**	Y***	N**	<u>FHWA / OES approval dates</u>
Cultural Resources Areas					
Sites eligible and/or listed for the NRHP		X			
Section 4(f) Determination of No-Use					
Programmatic Section 4(f)					
Individual Section 4(f) Evaluation					

Remarks: LJB reviewed available project mapping including the USGS 7.5 Minute Topographic Map, the Clermont County issued map and aerial and satellite imagery, and performed field reviews, but did not find any parks, natural, wildlife, and waterfowl refuges, or other recreational land within the project limits or the immediate project vicinity. The project area is comprised of strip commercial, retail and service fronting on State Route 28, with residential behind the strip development. Since there are no parks, refuges, or other recreational land within the project, no Section 4(f) resources with respect to parks, refuges, or other recreational land will be impacted by the proposed project. See page 18 of Appendix A for aerial photo showing the project area. Representative photographs of the project area can be found in Appendix C starting on page 72.

LJB reviewed the National Parks Service's Section 6(f) website on April 15, 2008 and confirmed that there are no 6(f) resources in the project area. See page 59 in Appendix B.

No historic properties will be affected by the project (see "Section C – Cultural Resources" for a detailed discussion of this finding). Therefore, the project will have no Section 4(f)/Section 106 impact to cultural resources.

** If the resource is present but no impacts are anticipated, the reason why is described under Remarks.
 *** Any impacts, mitigation and agency coordination are described under Remarks and coordination letters are attached.
 ****If "No", discuss in the remarks section details about how this determination was made.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

SECTION E – AIR QUALITY & NOISE
--

Will the project move the travel lanes closer to sensitive areas?

Y	N
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Air Quality

	Y	N
Conformity Status of the Project		
Is the project in an air quality non-attainment or maintenance area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Criteria pollutant in non-attainment		
PM 2.5	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Ozone	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is this project in the STIP?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is this project in the most current MPO air quality conforming TIP?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If NO, is this project exempt from conformity analysis?	<input type="checkbox"/>	<input type="checkbox"/>

	Y	N
Project-Level Analysis and Impacts		
Has the project scope changed substantially since the conformity analysis?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If YES, will this change require a reevaluation of the MPO TIP conformity?	<input type="checkbox"/>	<input type="checkbox"/>
Is a PM 2.5 analysis required for this project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is an air toxics (MSAT) analysis required for this project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Type of Analysis: Qualitative <input type="checkbox"/> Quantitative <input type="checkbox"/>		

Remarks: The U.S. EPA AirData system, located at <http://www.epa.gov/air/data/nonat.html?st-OH-Ohio>, the entire State of Ohio is in conformity for 8-Hour Ozone, however Clermont County is a non-attainment area for PM2.5. .

The project has been included in ODOT’s 2008-2011 Statewide Transportation Improvement Program (STIP), which lists the air quality status for this project as “Non-Exempt.” The Ohio, Kentucky and Indiana Regional Council of Governments (OKI) has included this project on their regional 2030 Regional Transportation Plan and 2008-2011 Transportation Improvement Program (TIP). See page 30 and 31 in Appendix B for the OKI project listings.

(See page 11-A for a continuation of this section)

Noise

	Y	N
Is a noise analysis required in accordance with FHWA regulations and ODOT’s statewide noise abatement policy?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If YES, is a design year noise impact predicted?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If YES, have all noise attenuation measures been considered, consistent with the policy?	<input type="checkbox"/>	<input type="checkbox"/>
If NO, explain why not: <u>No future noise impacts are predicted in the design year build condition.</u>		
Is noise attenuation found to be reasonable and feasible?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: Noise sensitive land uses are present within 600 feet the proposed project – 600 feet defines the traffic noise screening area. Theilman’s Manufactured Home Community is located south of the project area between Castleberry Court and Old Bank Road, with approximately 35 home sites within the screening area. Five single family residences located on Observation Court north of the project area and one isolated residence on Milford Hills Drive fall within the screening area. Finally, seven buildings containing 72 dwelling units in the Edgcombe Garden Apartment complex south of the project area are within the screening area. Each of these noise sensitive land uses include outdoor areas of frequent use. See page 23 in Appendix A for the location of the above mentioned sites.

LJB Inc. conducted a Traffic Noise Impact Screening that was submitted to ODOT Office of Environmental Services (OES) on April 6, 2009. Field measurements conducted by LJB and TNM LookUp output, based upon 2031 design year traffic under the build condition, show that no existing traffic noise impacts, as defined by ODOT and FHWA policies, are present at noise sensitive land uses within the traffic noise screening area.

(See page 11-A for a continuation of this section)

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Air Quality (continued from page 11)

The proposed project is focused on modifications that will address delay and congestion issues along the corridor. Some of the proposed improvements include treatments that will improve the operation of major intersections along the corridor, which is expected to have a positive effect on air quality by reducing congestion and the associated vehicle emissions.

PM 2.5

On August 19, 2008 ODOT OES sent a letter and project information to the Federal Highway Administration (FHWA) requesting a project level PM 2.5 and PM 10 conformity determination for several project in the 2008-2011 STIP, including the CLE-28-2.05 project. On September 4, 2008 FHWA responded stating, "Based upon our review of the project listing and through consultation with FTA and USEPA we find the projects on the attached list are not projects of air quality concerns and that they have met the statutory requirements of the Clean Air Act and are exempt from further hot-spot analysis per 40 CFR 93.116(a). This letter is included on page 60 to 62 in Appendix B.

Mobile Air Source Toxics

This project will not result in any meaningful changes in traffic volumes, vehicle mix, location of the existing facility, or any other factor that would cause an increase in emissions impacts relative to the no-build alternative. As such, FHWA has determined that this project will generate minimal air quality impacts for Clean Air Act criteria pollutants and has not been linked with any special MSAT concerns. Consequently, this effort is exempt from analysis for MSAT. Page 63 in Appendix B provides ODOT OES guidance regarding MSAT compliance.

Noise (continued from page 11)

In addition, the TNM LookUp results for the design year (2031) also show that no future traffic noise impacts are predicted at those same locations, as noise levels are below the Noise Abatement Criteria threshold. Since observed and modeled traffic noise levels do result in existing traffic noise impacts, and impacts are not predicted for the future under the build condition there are no future noise impacts predicted in the design year build condition.

In an IOC dated April 8, 2009, the ODOT Office of Environmental Services stated, "The subject Noise Screening report has been reviewed by this office and we concur with the consultant's conclusion that no additional traffic noise analysis is required for this project, since there are no future noise impacts predicted in the design year build condition. The IOC is included on page 64 in Appendix B.

This is page 11-A of 15, which is part of: Categorical Exclusion Level 2 Date: 11/13/2009

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

SECTION F – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?
 Will the proposed action result in substantial impacts to community cohesion?
 Will the proposed action result in substantial impacts to local tax base or property values?

Y	N
X	
	X
	X

Remarks: The City of Milford developed an access management plan in August of 2000 for the State Route 28 corridor to address traffic congestion. This plan was the basis for the access control treatments proposed in this project, which include the consolidation of access drives for two commercial developments, as shown with red 'X's on the graphic provided on page 19 in Appendix A. The commercial developments containing these three access drives have at least two other drives providing direct access onto State Route 28 – see the proposed improvements identifying the driveway locations on page 19 in Appendix A. And while some strips of right of way will be required from the frontage of properties adjacent to the project corridor, impacts and right-of-way acquisition from the adjacent businesses will be minimal, with no whole acquisitions or relocations necessary. With the proposed changes, the project is expected to support local development patterns and have a positive impact for property values and business along this corridor by addressing congestion issues and providing new sidewalks for pedestrian access to businesses.

(See page 12-A for a continuation of this section)

Will the proposed action result in reasonably foreseeable secondary or cumulative impacts?

Y	N
	X

Remarks: No secondary or cumulative impacts are expected to result from this project.

Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public utilities, fire, police, emergency services, religious institutions, public transportation or pedestrian and bicycle facilities?

Y	N
	X

Remarks: The proposed roadway project will not require road closures during construction. No negative impacts to health and educational facilities, public utilities, fire, police, emergency services, religious institutions, are expected. Bus route maps made available by the Southwest Ohio Regional Transit Authority do not show any public transportation service on this section of State Route 28, and currently no pedestrian or bicycle specific facilities are present in the project area. The project is expected to benefit response time by police, fire and EMS by reducing congestion in the corridor, and will construct sidewalks on both sides of State Route 28 to provide pedestrian access to the commercial land uses.

Environmental Justice (Presidential Executive Order 12898)

During public involvement activities, were Environmental Justice issues raised?
 Are any Environmental Justice populations located within the project area?
 Will the project result in adversely high or disproportionate impacts to the population?

Y	N
	X
X	
	X

Remarks: The area surrounding this project is home to moderate concentrations of Environmental Justice (minority or low income) populations. Pages 28 and 29 in Appendix A provide mapping developed with 2000 U.S. Census data that present the composition of the residential population in the general area of the project. As the mapping shows, individuals in poverty comprise less than eight percent of the population in the census tracts surrounding the project area, and those considered minorities comprise six percent or less of the surrounding population.

Generally, the project is expected to provide positive impacts for local populations, including any area Environmental Justice populations, as the project will provide an improved roadway that will help reduce congestion, and install sidewalks throughout the corridor.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Regional, Neighborhood, and Community Factors (continued from page 12)

With the addition of sidewalks to project area of State Route 28, community cohesion is anticipated to be positively affected. Existing State Route 28 has no sidewalks within the project area between McClelland Road and Castleberry Road. Several residential developments and single family homes are present near the project area, behind the commercial strips along State Route 28. Behind the commercial strip south of State Route 28 exists a manufactured home park (Theilman’s Manufactured Home Community) that is accessed from Old Bank Road and Castleberry Court. Behind the commercial strip north of State Route 28 exists large residential areas that are accessed from McClelland Road and Milford Hills Drive. Sidewalks are present within the mobile home park and along McClelland Road and the residential areas. The proposed sidewalks are intended to provide safe, non-motorized access for residents to the developed commercial area along State Route 28.

Data from the U.S. Census Bureau and the U.S. EPA was examined to assess the presence in the project area of populations protected under Title IV of the Civil Rights Act of 1964. This data was evaluated for concentrations of the elderly, the disabled, populations who speak little or no English, and households with no automobiles that may be affected by the project. None of these populations represent a high concentration of Title IV protected populations that would be directly affected by the proposed project, as the adjacent land use along this section of State Route 28 is entirely commercial (not residential). Pages 24 to 27 in Appendix A provide mapping related to Title IV population in the project area. The Census tract to the south of the project area does contain a higher percentages of elderly, disabled, and non-automobile owners, but the percentage does not exceed 25 percent of the total population, and the residential units housing those populations are not immediately adjacent to the project area. Providing sidewalks along both sides of the road, designed to Americans with Disabilities compliant design standards, should enhances mobility options for these populations that tend to have lower rates of automobile use compared to the general public, by providing safer pedestrian access to businesses along the corridor.

This project is not expected to affect community cohesion, as it will improve an existing transportation facility on its existing alignment, and will not require any total property acquisition or relocations. The project will also provide sidewalks on both sides of State Route 28 along the entire project corridor – currently no sidewalk exists.

The ODOT/OES “Community Impact Assessment Checklist” that was completed for this project is provided on pages 65 to 68 in Appendix B.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Displacement of People, Businesses or Farms:

Will the proposed action displace people, businesses or farms?

Y	N
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Number of displacements: Residences: Businesses: Farms: Institutions:

Remarks: The project will require only strip takes of right of way, primarily from commercial properties. No full acquisitions or relocations will occur. Where permanent right of way is required, the approximate average width of the permanent acquisition is 23 feet.

SECTION G – PUBLIC INVOLVEMENT

Per ORC 5511.01 and 23 CFR 771.111 (h)(2)(i) and (ii), every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Discuss what public involvement activities (letters to affected property owners and residents, meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Were you inclusive of minority and low income people in your public involvement activities? **Yes*** **No**
** If YES, explain below.*

The proposed project has been introduced to those involved with the project, both directly and indirectly, on numerous occasions. A brief summary of project contact with the public follows:

- April 18, 2008: The Clermont County Transportation Improvement District (CCTID) sent 43 project right-of-entry letters to property owners, tenants and residents located within and immediately adjacent to the project roadway.
- November 17, 2008: The CCTID distributed invitation fliers to 47 project stakeholders to provide project information and encourage their attendance at the December 11, 2008 project public involvement meeting.
- November 18, 2008: The Council of the City of Milford included the upcoming public involvement meeting for the State Route 28 project in their meeting minutes.
- November 26, 2008: The Board of County Commissioners of Clermont County distributed a news release of the upcoming public involvement meeting for the State Route 28 project.
- November 26, 2008: The news release was placed on the ClermontCountyOhio.gov web site.
- The meeting was mentioned on TV channels 5 and 12. The meeting notice was run in the Community Press and Cincinnati Enquirer.
- The December 11, 2008 public involvement meeting was videotaped for the purpose of future public television broadcasts.
- The Intercommunity Cable Regulatory Commission (I.C.R.C.) aired the recording of the December 11 public involvement meeting 10 times over 8 consecutive days during the month of January 2009.

(See page 13-A for a continuation of this section)

Public Controversy on Environmental Grounds

Will the project involve substantial controversy concerning community and/or natural resource impacts?

Y	N
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: No public controversy has been identified nor anticipated as part of this project.

This is page 13 of 15, which is part of: Categorical Exclusion Level 2 Date: 11/13/2009

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Public Involvement (continued from page 13)

The public meeting was held by the CCTID on December 11, 2009 at the Milford City Hall (approximately ½ mile west of the project) from 5:30 to 7:00 PM. The meeting was held in an “open house” format. Twenty nine (29) citizens attended, which included one person from the city of Milford, three representing Miami Township, one with ODOT District 8, and one from the Board of County Commissioners. A project fact sheet and comment sheet were distributed to those in attendance. A preliminary set of plans was available at the meeting for property owners/tenants with specific concerns. There was no sign of controversy during the meeting.

Although the public was encouraged to fill out their comment sheet, only two comments were received as a result of the public involvement. Both commenters requested dedicated right-turn lanes on the southbound leg of the McClelland Road intersection with State Route 28. The Clermont County Engineer’s Office developed a response to the request that included the statement, “Our evaluation of the McClelland intersection with the planned improvements indicate that the traffic capacity analysis for the proposed improvements to not support the need nor justify a southbound right turn lane on McClelland Road.” No additional project comments have since been received.

All public involvement documentation is provided on pages 72 to 89 in Appendix D.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

	<u>Documentation</u>	
	Y	N
Environmental Site Assessment Screening / Checklist	X	
Phase I Environmental Site Assessment	X	
Phase II Environmental Site Assessment	X	
Design for Remediation		X

Remarks: LJB Inc. submitted an Environmental Site Assessment (ESA) screening report to ODOT District 8 on November 25, 2008. The report recommended Phase I ESA investigations for four businesses. On January 5, 2009, ODOT OES provided their concurrence with the recommendation to investigate the four businesses identified in the report. A copy of the correspondence providing this direction is provided in on page 69 in Appendix B.

The Phase I ESA was prepared by LJB on April 17, 2009. The report recommended 'no further investigations' for the Thorton's Gas Station property, Former Classic Collision Repair, and the Milford Oil property. A Phase II ESA was recommended at the Hills Real Estate Group property.

(See page 14-A for a continuation of this section)

SECTION I – PERMITS CHECKLIST

	<u>Required</u>	<u>Not Required</u>	<u>Complete</u>	
			Y	N
			X	
OES Permit Determination (PD)			X	
Army Corps of Engineers (404/Section10 Permit				
Individual (IP)		X		
Nationwide (NWP)	X			
Pre-Construction Notification (PCN)		X		
OEPA				
Level 1 Review – General Isolated Wetland Permit		X		
Level 2 Review – Individual Isolated Wetland Permit		X		
Level 3 Review – Individual Isolated Wetland Permit		X		
401 Water Quality Certification (WQC)		X		
NPDES Construction Storm Water Permit	X			
US Coast Guard Section 9 Bridge Permit			X	
Wetland and/or Stream Mitigation			X	
Flood Plain Permit			X	

Remarks: The proposed project will impact at most 60 feet of ephemeral stream and no wetland. Also, no temporary fill will be needed. The ODOT OES determined in an IOC dated June 19, 2009 that the proposed project activity is covered under a Nationwide Permit #14. This permit and its conditions will be included in the project plans as a special provision. This verification is valid until the NWPs expire on March 18th, 2012. The Special Provisions Package (SPP) must be attached to the construction plans and kept on the project site until the project is complete. All special provisions set forth within the permit package will be followed for the duration of the project. The signed 401/404 permit determination IOC is included on page 68 in Appendix B.

National Pollutant Discharge Elimination System (NPDES) Construction Storm Water Permit
 Because the proposed project will cause over one acre of earth-disturbing activities, a note will be included in the design plans requiring the contractor submit a Notice of Intent to the Ohio EPA for coverage under the NPDES construction storm water permit. As required by the permit, a Storm Water Pollution Prevention Plan must be developed for the project.

(See page 14-A for a continuation of this section)

Form version: 11/16/044

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Section H: Hazardous Materials and Regulated Substances (continued from page 14)

Following review of the Phase I ESA, OES responded in a July 11, 2009 IOC with the following recommendations.

- A Phase II ESA is warranted for the Hills Real Estate Group property (749 State Route 28). Three (3) soil borings should be advanced within the new right-of-way to a depth of 10 feet and sampled continuously. One soil sample with the highest field screen reading from each boring will be submitted for laboratory chemical analysis. A plan map indicating the proposed boring locations was attached with the IOC.
- Based on the lab results for soil found in the BUSTR files, a plan note for petroleum contaminated soil (PCS) should be developed and placed into the plans for the Thorton’s Gas Station (752 & 798 State Route 28) and Milford Oil Co. (709 State Route 28).
- Based on the lab results for groundwater results found in the BUSTR file, a plan note for regulated water should be developed and placed in the plans for the Thorton’s Gas Station (752 & 798 State Route 28) if dewatering is necessary in construction. In addition, low strength mortar should be used for backfill if any storm sewer work will be performed along this property.

A copy of this correspondence is provided on pages 70 and 71 in Appendix B

The Phase II ESA was conducted during the months of August and September 2009. The subsequent report is currently under review. Results of the assessment will be addressed within the project plan notes as needed. For the above noted sites, Petroleum Contaminated Soil plan note and Regulated Water plan note will be included in the construction plans and as an environmental commitment.

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

SECTION J – ENVIRONMENTAL COMMITMENTS MADE & RESOURCES TO BE AVOIDED

Maintenance of Traffic During Construction. The maintenance of traffic will require a minimum of one lane of traffic be maintained at all times during construction. Access will be maintained to all businesses at all times.

Construction Notification. The Contractor will advise the Project Engineer a minimum fourteen (14) days prior to the start of construction activities and/or any lane closures or detours. The Project Engineer will forward this information to the ODOT District 8 Public Information Officer (PIO) by fax at (513) 932-7651 or at the ODOT District 8 PIO notification website, <http://www.dot.state.oh.us/districts/D08/pages/PlanningPIOWebform.aspx>. The PIO will then provide written notification to the local emergency responders, schools, and businesses of any upcoming lane closures and/or detours.

Right of Way and Utility Involvement. All utility coordination required during construction will be detailed as notes in the plans. The project sponsor will be responsible for coordination with utility owners for utility relocations that need to occur prior to construction of the project. These utilities include Duke Energy (gas), Cincinnati Bell and Time Warner Cable (telecommunications).

Temporary Sediment and Erosion Control. ODOT's Supplemental Specification (SS) 832 will be listed on the Project Title Sheet. A plan note will also be included that will not permit the contract to use straw bales as a form of erosion control.

In-Stream Work Plan Note. One stream crosses the project area, located at STA 52+00 and traveling northwest. In-stream or stream crossing work should be conducted during low flow period (August 1 through October 31) or as follows. The Contractor will be allowed to begin proposed in-stream work outside the low-flow dates only when the stream is dry and there is no forecasted precipitation for the next 48 hours. Work shall be completed within 48 hours if conducted outside the low-flow dates. The schedule of this work shall be approved by the Project Engineer prior to commencement. Any disturbed areas in the stream beds should be returned to pre-construction contours. Rip-rap used should be kept to the minimum amount needed to prevent scour and shall consist of clean rock only (free of any toxic or fine material). All rip-rap shall be a minimum of three inches in diameter and be washed to remove fine particulate matter (clay, silt, sand and soil). Any temporary fill should be completely removed from the streambed immediately upon completion of in-stream work.

(See page 15-A for a continuation of this section)

SECTION K – CONCURRENCE

It is hereby determined that the subject project meets the criteria for CE in accordance with the *Programmatic Categorical Exclusion Agreement* between ODOT and FHWA. This action does not: induce significant impacts to planned growth or land use for the area; require relocation of significant numbers of people; have significant impact on any natural, cultural, recreational, historic, or other resource; involve significant air, noise, or water quality impacts; have significant impacts on travel patterns; or otherwise, either individually or cumulatively, have any significant impacts and do not require the preparation of an Environmental Assessment or an Environmental Impact Statement.

As supported by information contained in this Categorical Exclusion Document, this project qualifies for a CE Level 2, Item Number 2 & 10, in accordance with the *Programmatic Categorical Exclusion Agreement* between ODOT and FHWA dated March 6, 2003.

David Newhouse, LJB Inc.	Nicole A. Clune, P.E., LJB Inc.
Name of Preparer and Organization	Date
District Environmental Coordinator	11/13/2009
Date	District Planning & Programs Administrator
Office of Env. Services Administrator	If CE-4: Date Submitted to FHWA
Date	

This is page 15 of 15, which is part of: Categorical Exclusion Level 2 Date: 11/13/2009

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Section J: Environmental Commitments Made & Resources to be Avoided (continued from page 14)

Vegetation and Tree Replacement Plan Note. All streambank vegetation shall be left undisturbed to the maximum extent possible. Areas where vegetation is removed shall be re-vegetated with native tree species. Any disturbed streambanks shall be returned to previously existing contours and elevations. Trees shall be one inch in diameter and balled/ burlap nursery stock. After a full growing season for the trees, any stakes and guide wires shall be removed and properly disposed of. Any trees that die during the first growing season should be replaced.

National Pollutant Discharge Elimination System (NPDES) Construction Storm Water Permit Plan Note
Because the proposed project will cause over one acre of earth-disturbing activities, a note will be included in the design plans requiring the contractor submit a Notice of Intent to the Ohio EPA for coverage under the NPDES construction storm water permit. As required by the permit, a Storm Water Pollution Prevention Plan must be developed for the project.

Phase II ESA at the Hills Real Estate Group property (749 State Route 28). The Phase II ESA is being conducted during the months of August and September 2009. Results of the assessment will be addressed within the project plan notes as needed.

Plan Note for Hazardous Material Storage on Construction site. Idle equipment, petrochemicals and toxic/hazardous materials should not be stored near any drainage ways, ditches or streams that could convey such materials to the unnamed tributary of the Little Miami. Petrochemicals and toxic/hazardous materials should not be discharged into the unnamed tributary of the Little Miami or any drainage ways, ditches or streams. Refueling of equipment shall not occur near any drainage ways, ditches or streams. A spill containment and cleanup plan should be generated prior to the start of the project. *The above recommendations will be addressed by applying "Supplemental Specification 832-Temporary Sediment and Erosion Control" to the project plans*

Plan Notes for Regulated Water. *Item Special - Work Involving Regulated Water:* If excavations, within the limits of the Thorton's Gas Station property, require dewatering operations for construction purposes, the Contractor shall dewater and subsequently dispose of the said waters by methods approved by the Contractor. Any water containerized by the Contractor between the given property limits shall be subject to testing, according to USEPA requirements, by an inspector provided by the Engineer. If the Engineer determines that the water is regulated then the Contractor shall be responsible for disposing of the regulated water. The method of disposal will be approved by the Engineer. The work involved with this pay item includes the handling, storage, testing and disposal of regulated water. The Contractor shall furnish all of the labor, equipment and materials necessary to properly handle, store, test, transport and dispose of regulated materials. The Contractor shall obtain all necessary permits and/or necessary authorizations and fees needed to store, transport and dispose of the water in accordance with applicable local, state and federal regulations. One copy of each record shall be submitted to the Engineer. Payment for this work shall be made at the contract price bid per ton or cubic yard. The following estimated quantities have been included in the General Summary for the work noted above:

690M65020 Item Special - Work Involving Regulated Water XX ton

Plan Notes for Protection of Groundwater. To limit the potential for leachate migration, specifically for the water main installation in front of the Thorton's Gas Station (752-798 State Route 28) the bedding around the proposed pipe shall be comprised of Item 613 - Low strength Mortar (LSM) as per the 2008 ODOT CMS. The LSM shall extend from the bottom of the pipe trench to at least one level foot above the pipe. LSM may be used up to a depth of 1 foot below proposed grade. No granular materials will be used in backfilling of the water main/storm sewer work within this area. Materials removed during trench excavation may be reused provided the conditions of Items 203.03 and 703.16 of the 2008 ODOT CMS are met. Materials removed during trench excavation will be placed to a depth of approximately 1 foot below finished grade. For unpaved sections, the replaced waste materials will be topped with a 1-foot layer of clean fill, such that the excavated materials are re-capped following construction activities. Payment for the LSM will be included within the affected conduit's linear foot price.

This is page 15-A of 15, which is part of: Categorical Exclusion Level 2 Date: 11/13/2009

Ohio Department of Transportation

County CLE Route 28 Section 2.05 PID 82140 SJN 487184

Plan Note Petroleum Contaminated Soil (PCS). *Item Special - Work Involving Petroleum Contaminated Soils (PCS):* There is a potential for encountering petroleum contaminated soils along the frontage of the Milford Oil Company (located at 709 State Route 28, approximate beginning Station 46+00) and the Thorton's Gas Station (located at 752-798 State Route 28, approximate beginning Station 57+00). The Contractor must handle all excavated materials within the given locations in accordance with the following notes:

The Contractor shall provide the Engineer with five (5) days notice prior to beginning any excavation within the aforementioned limits to permit arranging for the necessary testing services. The Contractor shall provide the inspector. All material excavated by the Contractor within these limits during construction shall be subject to testing by first field screening the excavated material for petroleum contamination using an organic vapor analyzer (OVA). The excavated material, which exhibits petroleum contamination shall be stockpiled and segregated while samples of the material are analyzed by an Independent laboratory. Field screening results and visual observation will be the basis for segregating the excavated material. All tested material exceeding levels set forth by the Ohio EPA Petroleum Contaminated Soil Policy shall be treated as PCS. If the excavated material meets the conditions as defined by Section 203.03(J) of the 2008 CMS, the material may be used as backfill for other project purposes.

All material excavated by the Contractor within these specified locations which is determined to be potentially contaminated with petroleum substances, shall be stockpiled in an area provided by the Contractor and approved by the Engineer. The Contractor shall stockpile the material in a leak proof, covered container provided by the Contractor. The material shall remain on site until analytical sample results are received by the Engineer. The Engineer may permit temporary storage of suspected contaminated soils on an impermeable membrane. The membrane should be surrounded by bales of straw to prevent the suspect soils from coming into contact with the original soils. An impermeable membrane shall be placed over the stockpile to prevent contact with precipitation and/or surface runoff.

The Contractor is responsible for insuring that all of this excavated material is properly, tested, transported and disposed of in a licensed (by the local health department) and permitted (by the Ohio Environmental Protection Agency) solid waste facility or a Petroleum Contaminated Soil Remediation Facility (PCSRF) for proper disposal or remediation. Prior to disposal the Contractor shall contact the proposed facility to determine if any additional tests are required for disposal or remediation by the selected facility.

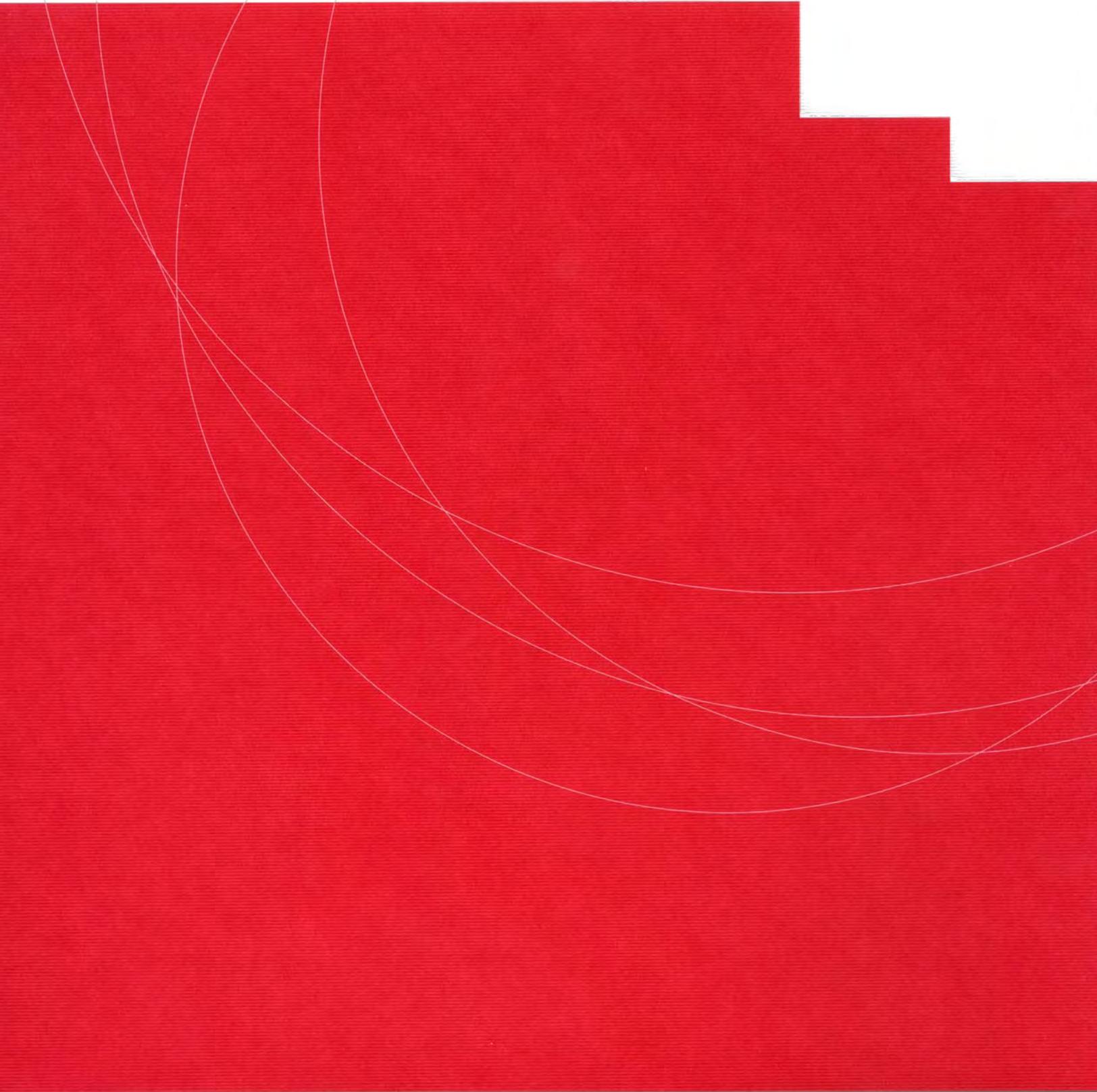
The Contractor shall furnish all labor, equipment and materials necessary to properly handle, test, store, transport and dispose of all regulated materials including any required permits, approvals or fees for the properties specified. Payment for this work shall be made at the contract price bid per ton or cubic yard. The following estimated quantities have been included in the General Summary for the work noted above:

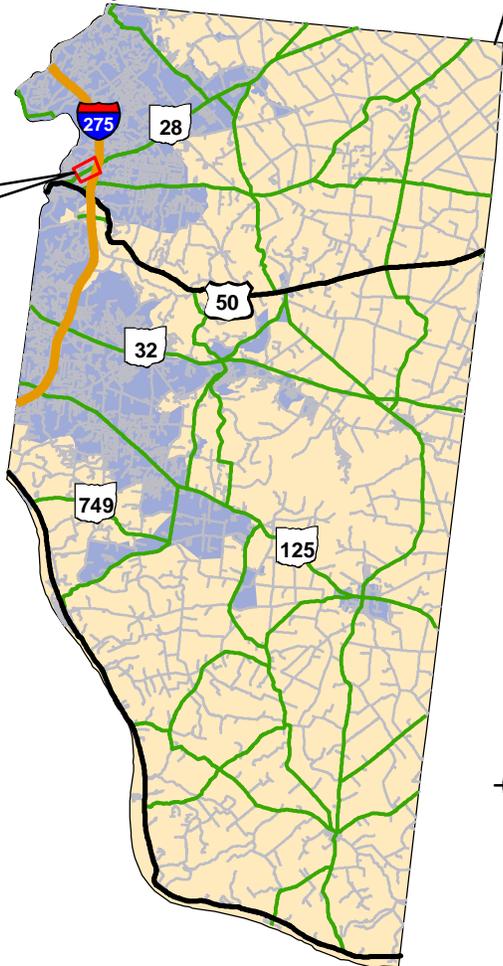
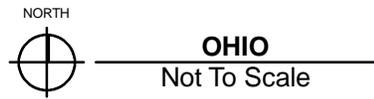
Item Special - Work Involving Petroleum Contaminated Soils XX ton.

Army Corps of Engineers Nationwide Permit and Special Provision Package. The proposed project activity is covered under a Nationwide Permit #14. This permit and its conditions will be included in the project plans as a special provision. The SPP must be attached to the construction plans and kept on the project site until the project is complete. All special provisions set forth within the permit package will be followed for the duration of the project.

This is page 15-B of 15, which is part of: Categorical Exclusion Level 2 Date: 11/13/2009

**Appendix A
Project Mapping**

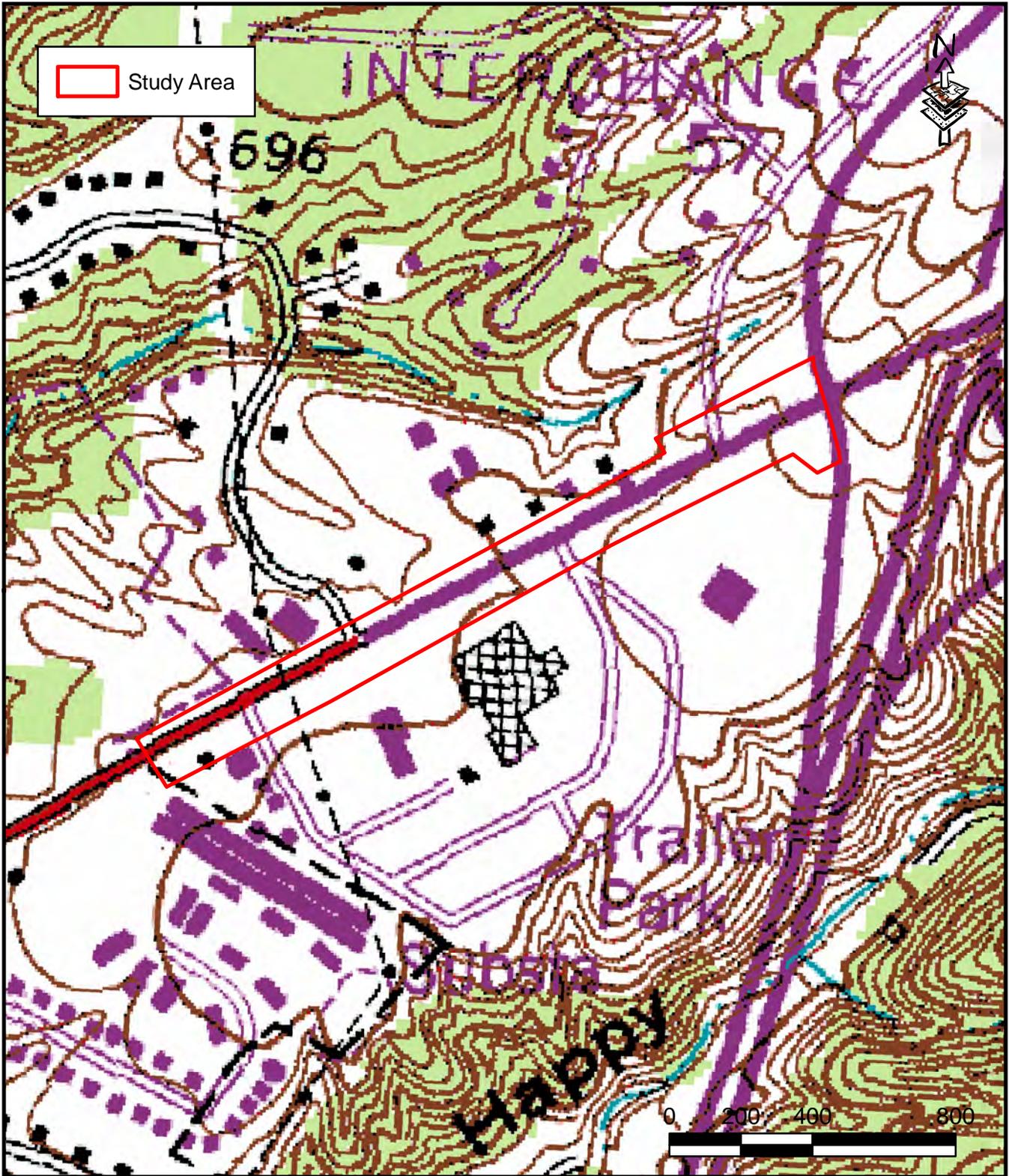





LJB Inc. • 3100 Research Blvd. • P.O. Box 20246
 Dayton, OH 45420-0246
 (937) 259-5000 tel • (937) 259-5100 fax • ljbinc.com

Project Location Map

CLE-28-2.05: Castleberry Court to I-275
Milford, Miami Township, Clermont County, Ohio




 LJB Inc. • 3100 Research Blvd. • P.O. Box 20246
 Dayton, OH 45420-0246
 (937) 259-5000 tel • (937) 259-5100 fax • ljbin.com

USGS Maderia Quadrangle

CLE-28-2.05: Castleberry Court to I-275
 Milford, Miami Township, Clermont County, Ohio



streams
 Project Area

Stream 1

Castleberry Court

Milford Hills Dr

McClelland Road

Castleberry Court

Old Bank Road

0 50 100 200
 Feet

 LJB Inc. • 3100 Research Blvd. • P.O. Box 20246
 Dayton, OH 45420-0246
 (937) 259-5000 tel • (937) 259-5100 fax • ljbin.com

Clermont-28: Castleberry Court to I-275
 Milford, Clermont County, Ohio
 2006 Aerial



 Little Miami River Basin Sole Source Aquifer
 Project_area

source: Ohio Environmental Protection Agency



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Sole Source Aquifer

CLE-28-2.05, PID 82140



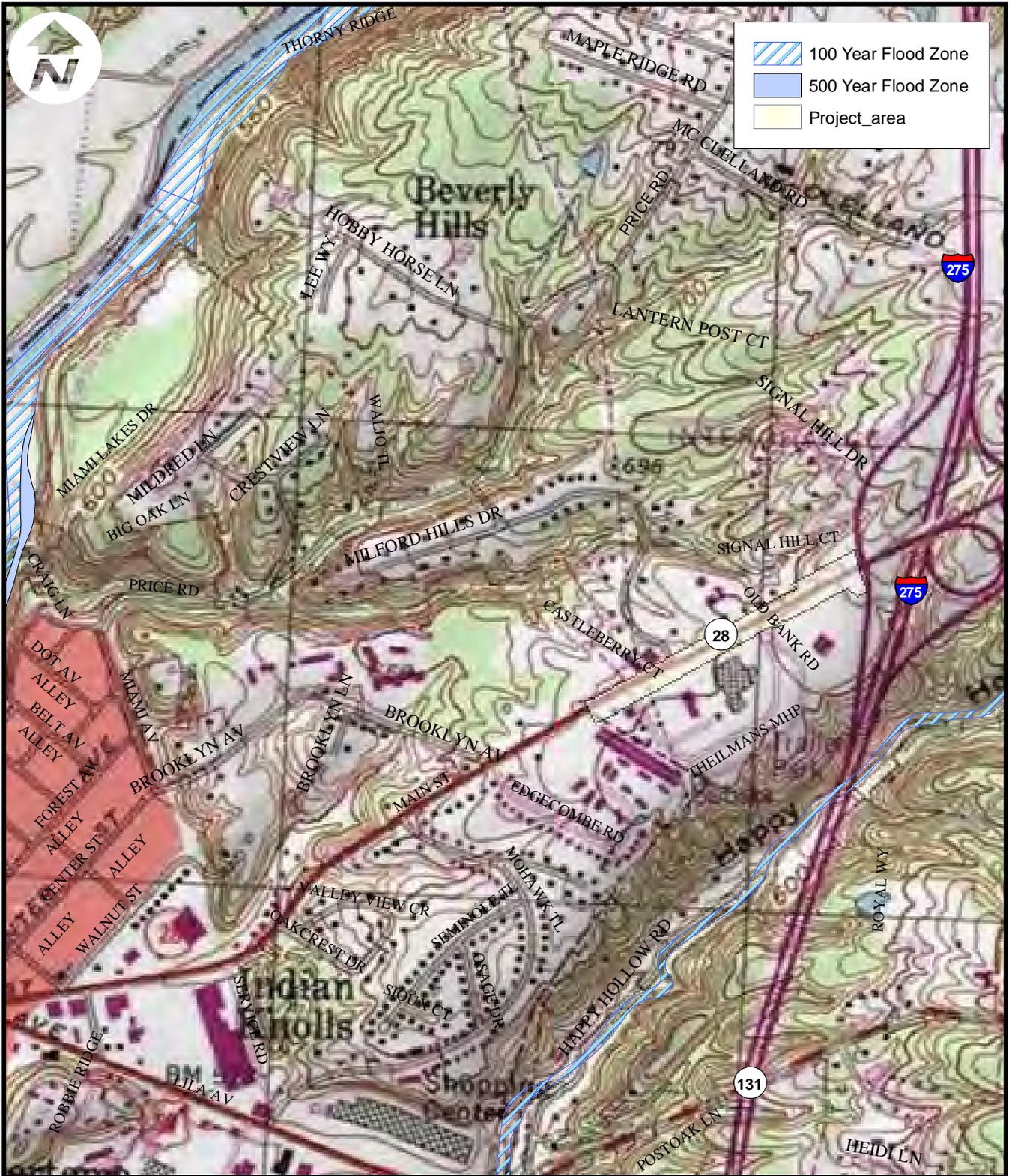
source: Ohio Environmental Protection Agency



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Public Water System Information

CLE-28-2.05, PID 82140



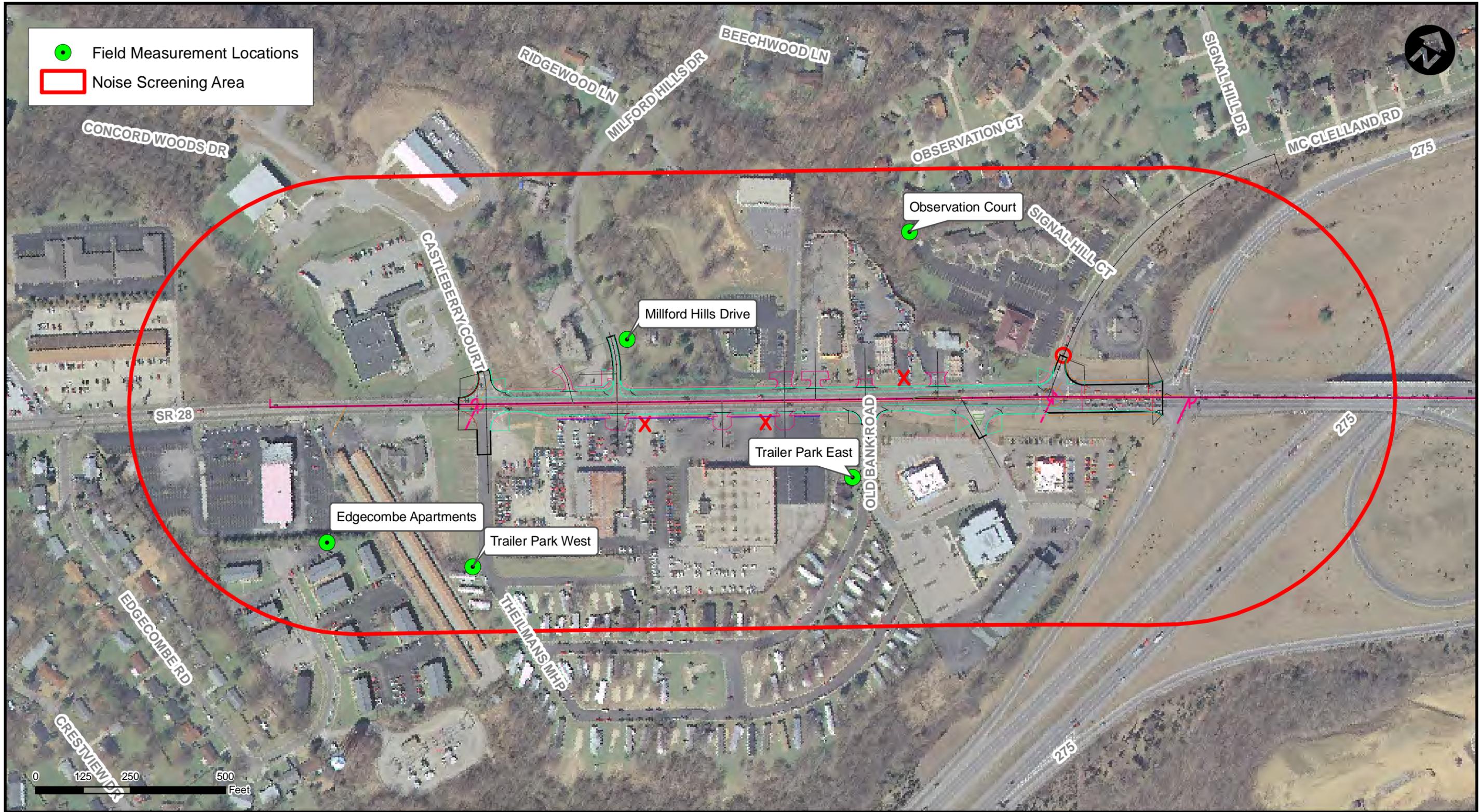
	100 Year Flood Zone
	500 Year Flood Zone
	Project_area



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FEMA Flood Zone Designations

CLE-28-2.05, PID 82140

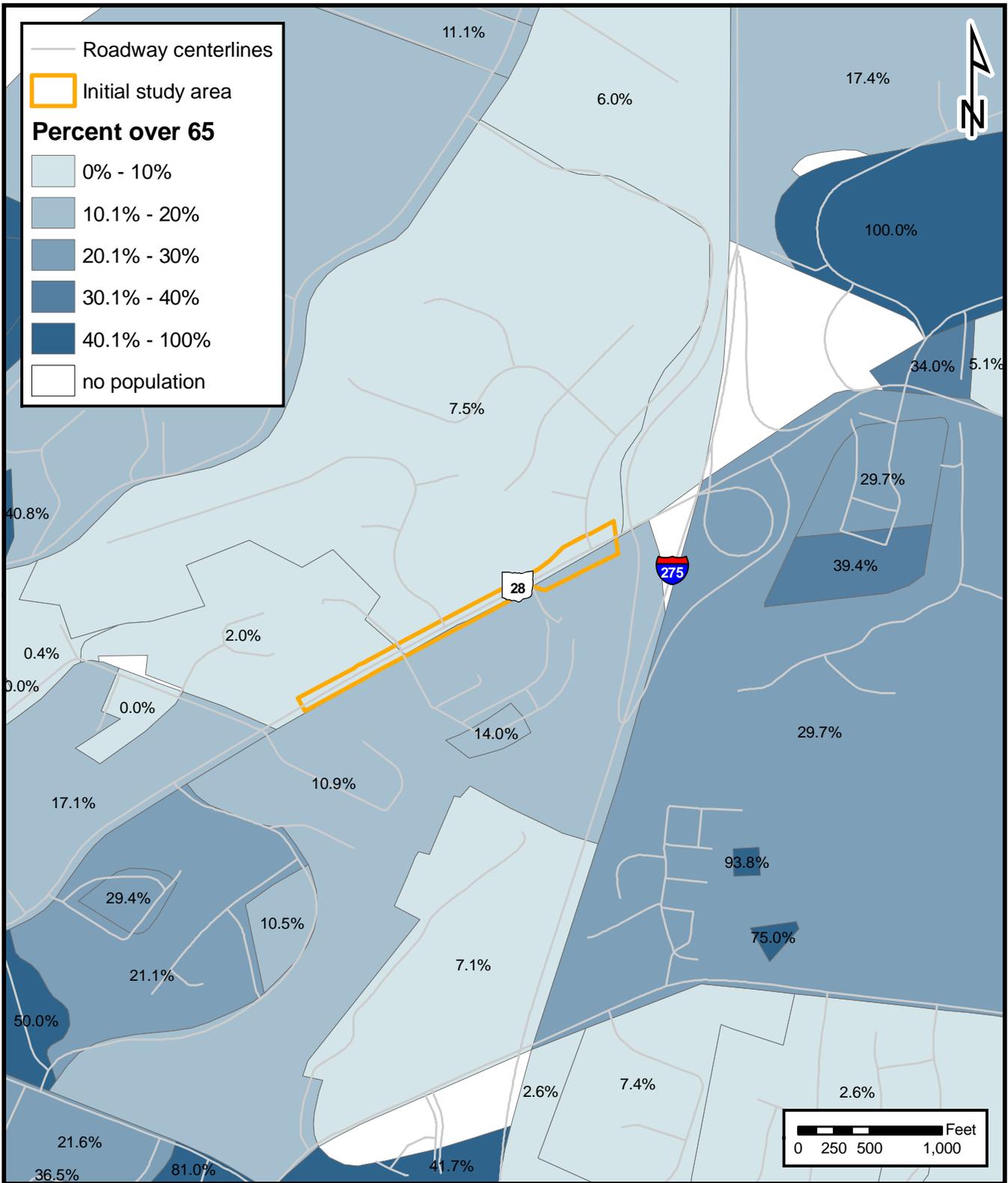


- Field Measurement Locations
- Noise Screening Area


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Traffic Noise Screening Area

Roadway Improvements
CLE-28-2.05, PID 82140



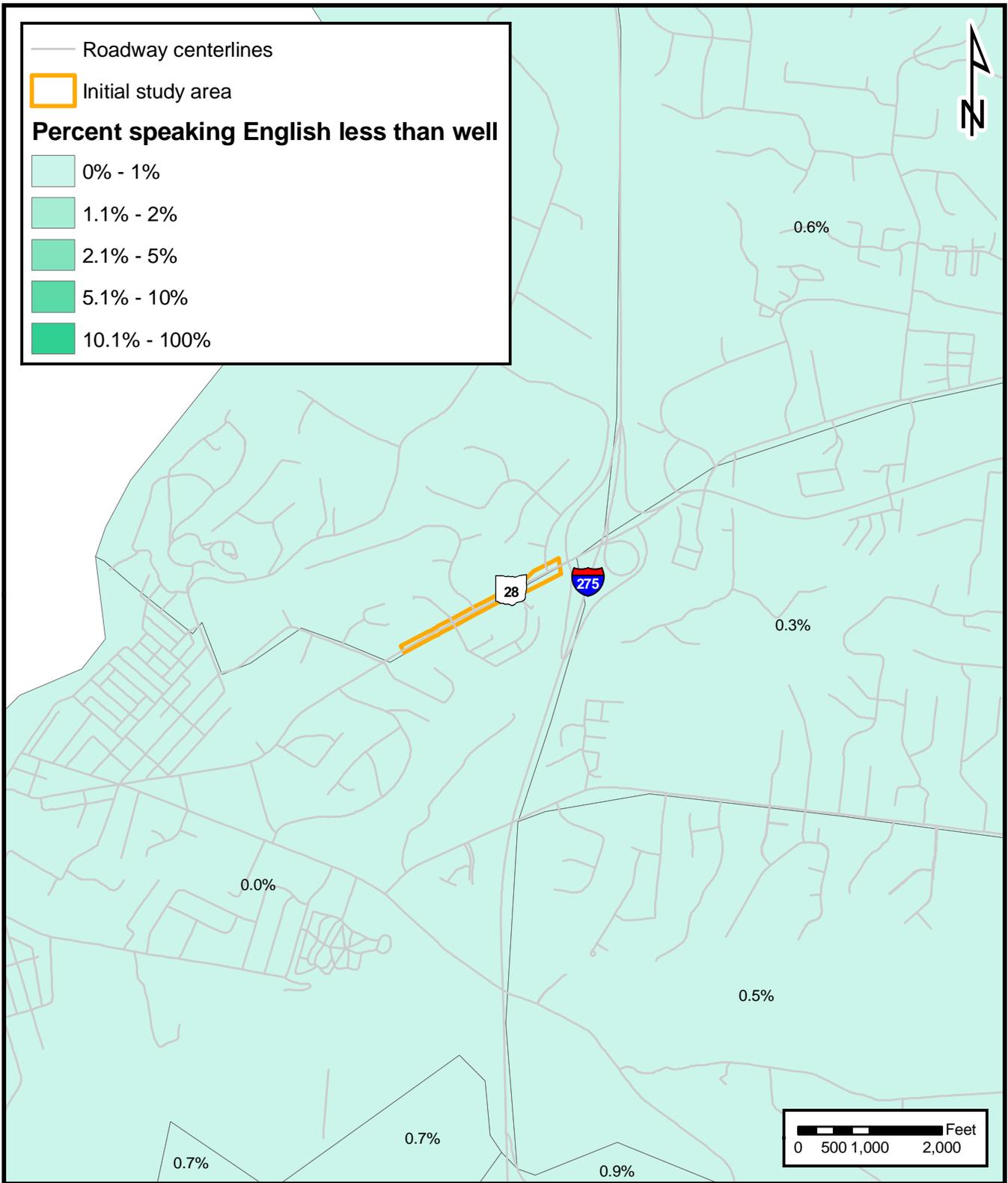
source: 2000 US Census



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Individuals Over 65, by Census Block

CLE-28-2.05, PID 82140



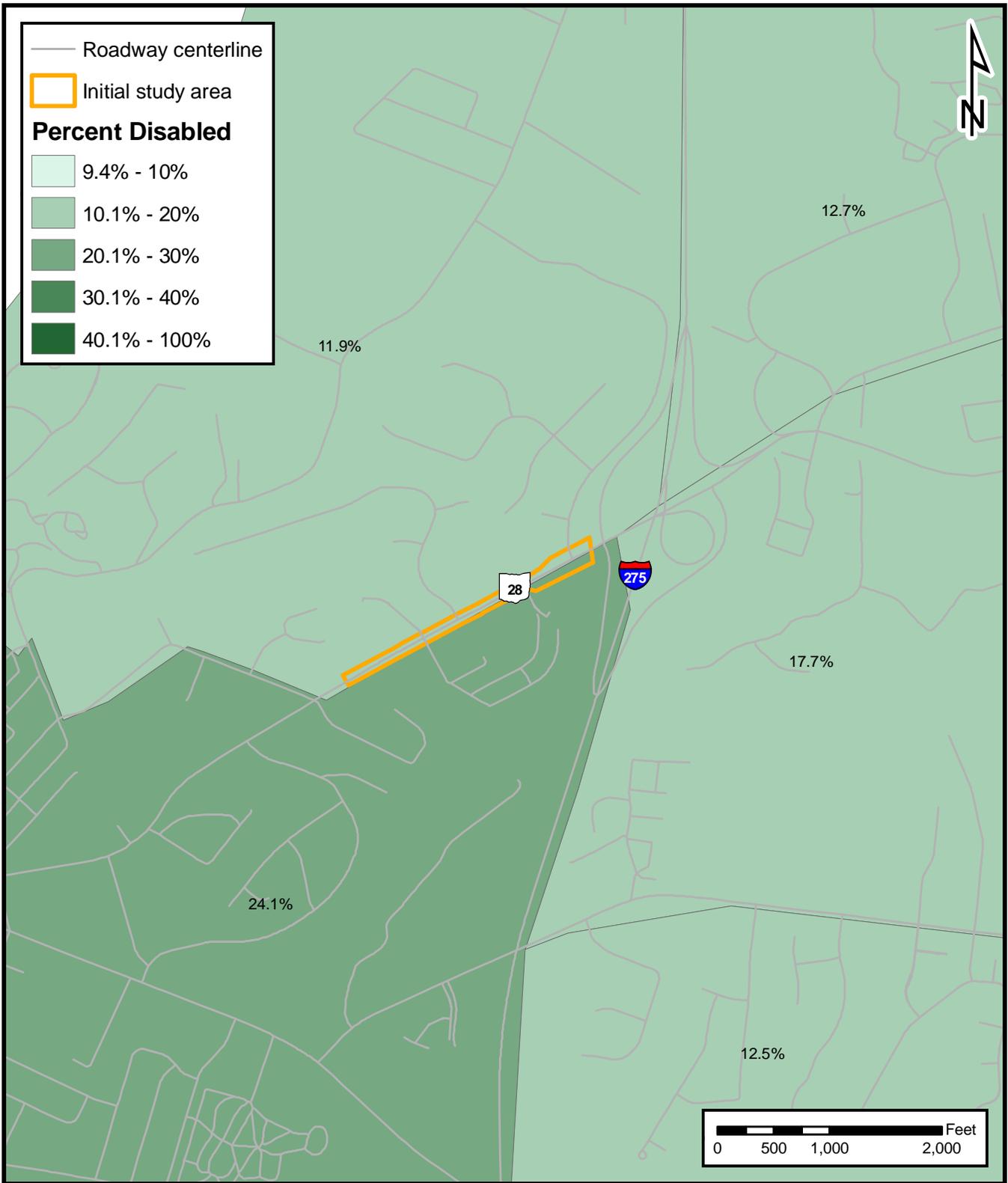
source: 2000 US Census



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Percent Speaking English
 Less Than Well by Census Tract

CLE-28-2.05, PID 82140



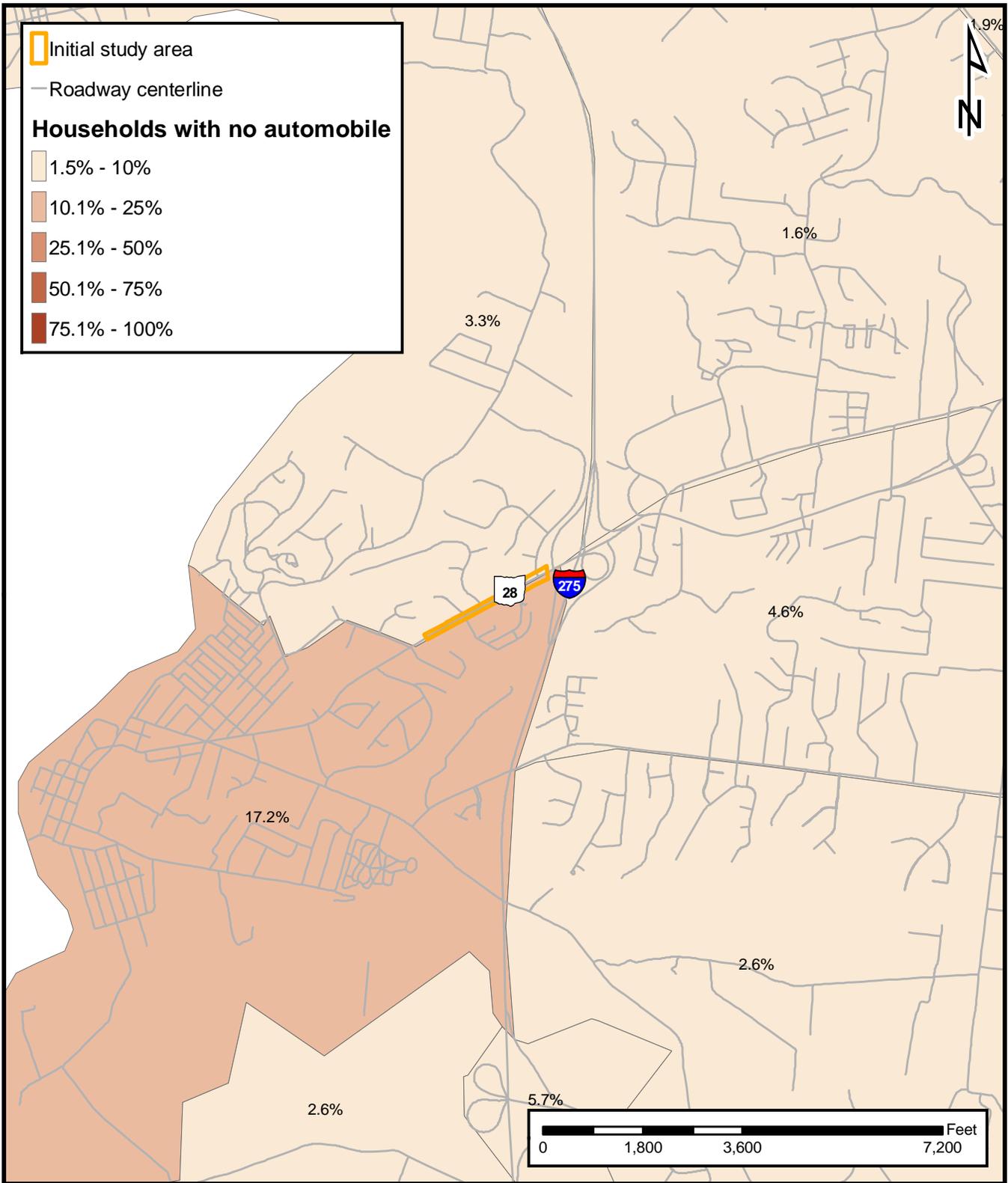
source: 2000 US Census



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Percent Disabled
 by Census Tract

CLE-28-2.05, PID 82140



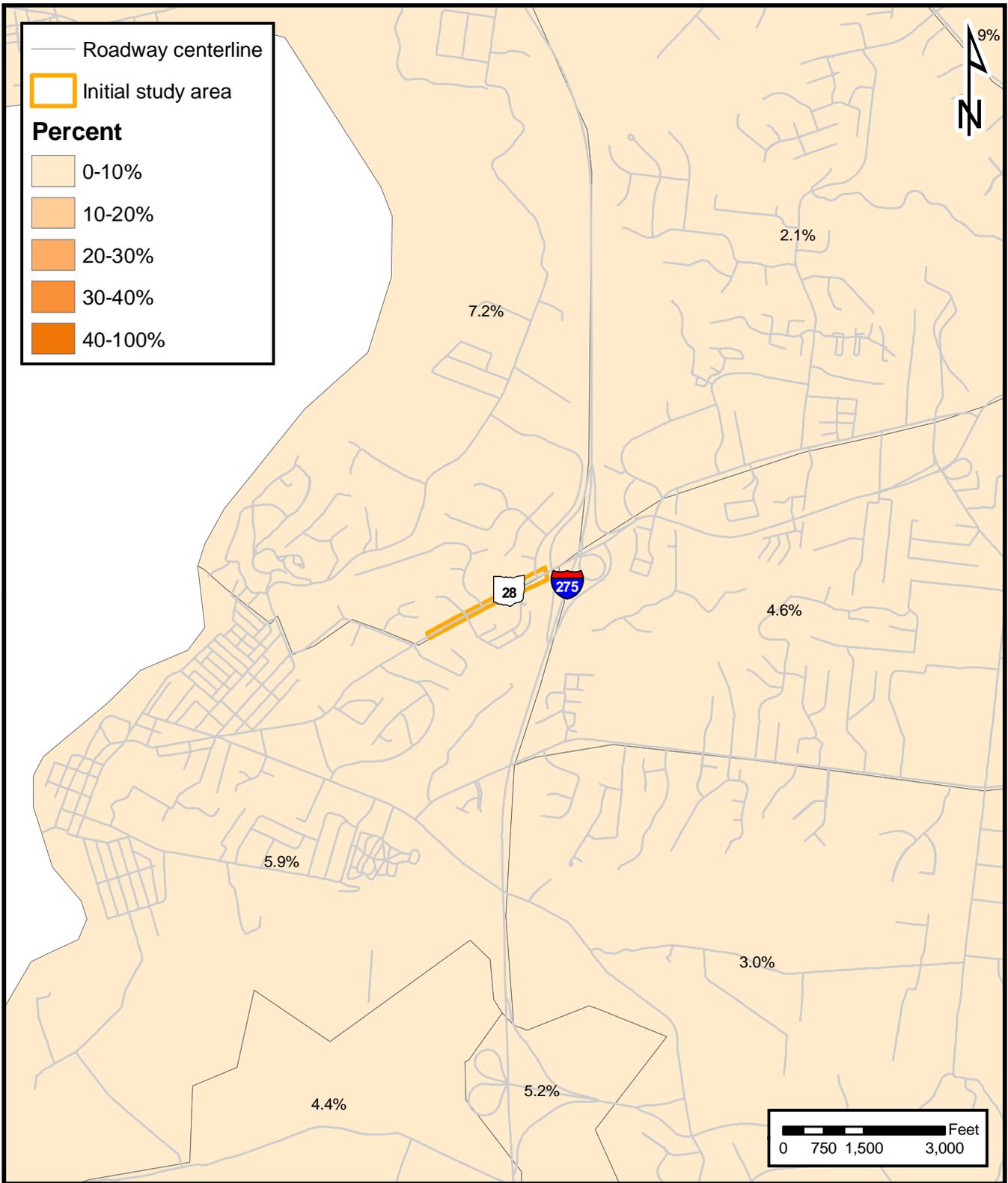
source: 2000 US Census



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Percent of Households w/no
Automobile by Census Tract

CLE-28-2.05, PID 82140

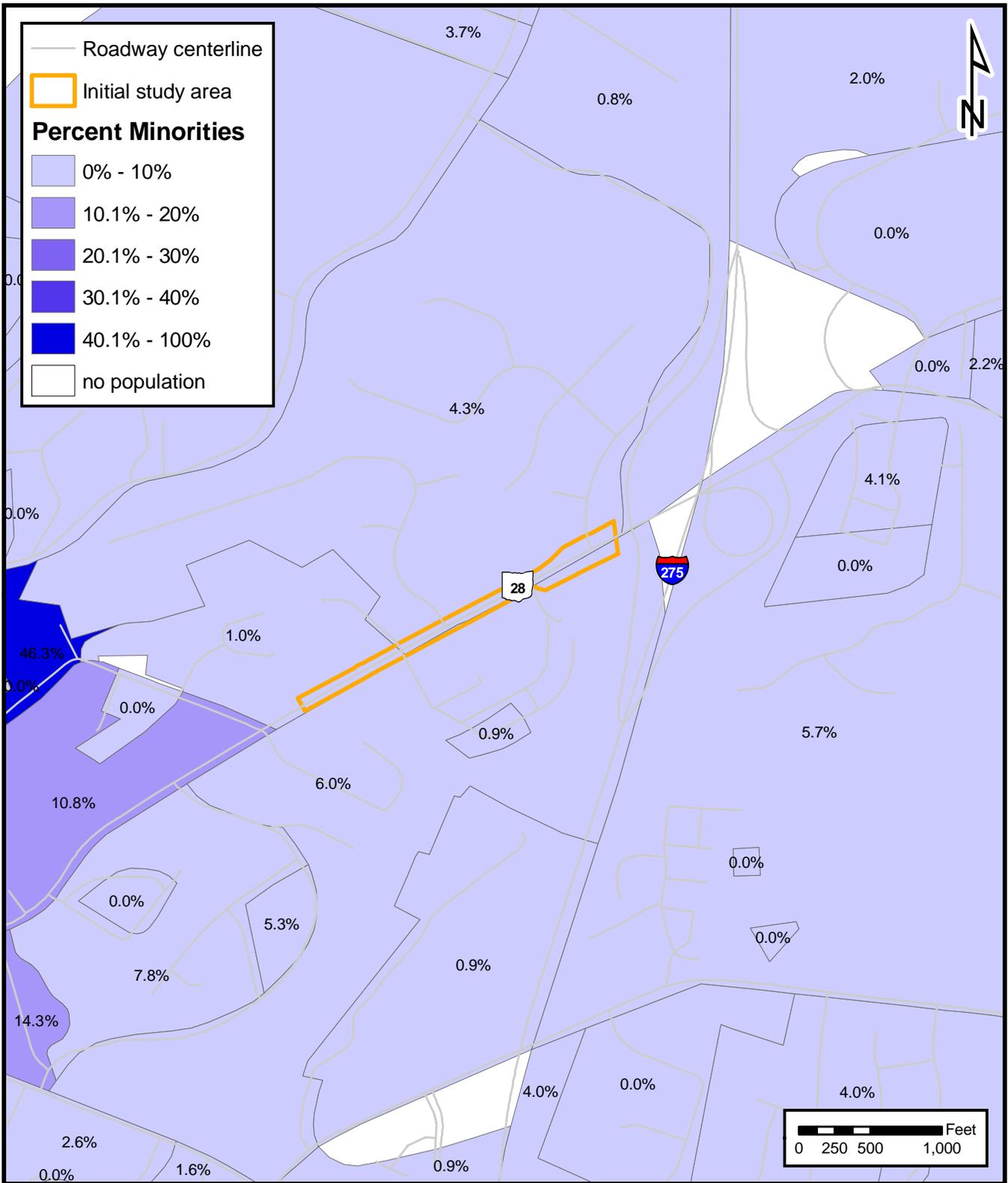


source: 2000 US Census



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Percent Individuals in Poverty
by Census Tract
CLE-28-2.05, PID 82140



source: 2000 US Census



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Minorities by Census Block, 2000

CLE-28-2.05
PID 82140



Appendix B
Resource Agency Coordination

Project ID	Plan ID	Facility	Location	Description	\$ Cost (M) (YOE)
82140		SR 28	From 0.44 miles west of IR 275 to IR 275 southbound exit ramp	Construct a five lane roadway (three lane existing) with a four-foot sidewalk on one side	3.0
79111		SR 28 Business	SR 28 to Cook Road	Widen to four through lanes with turn lanes at signalized intersections and landscaped median	7.6
22970-2	438	SR 32/Bach-Buxton Interchange	Elick Lane to Old SR 74	Extend five lane Bach-Buxton extension with SR 32 interchange	2.0
22970-1	440	SR 32/Glen Este-Withamsville Overpass	Glen Este-Withamsville Road	New Glen Este-Withamsville overpass	0.5
82588	417	SR 32-Batavia Interchange	SR 32 interchange in Village of Batavia	Convert existing half interchange to full	0.5
82587	445	SR 32-Harold Road	1000' west of existing Herold Road intersection on SR 32	New interchange	0.8
75303		SR 125	SR 125 and Amelia-Olive Branch Road	Intersection improvement/park-and-ride construction	4.7
2030 Plan					
82553	433	Aicholtz Road Connector	Mt. Carmel-Tobasco to Eastgate Boulevard	Reconnect Aicholtz Road under I-275 to Mt. Carmel-Tobasco Road	10.4*
82552	402	Aicholtz Road Extension	Glenn Este-Withamsville Road to Bach-Buxton Road	New connection between Glenn Este-Withamsville Road and Bach-Buxton Road	12.9*
82554	403	Aicholtz Road Widening	Eastgate Boulevard to Glen Este-Withamsville Road	Widen to five lanes with sidewalks	7.6*
82581	401	Amelia-Olive Branch Relocation	Clough Pike to Olive Branch-Stonelick Road at SR 32	New connector from Olive-Branch Stonelick Interchange to Clough Pike with sidewalks	5.1*
	447	Business 28-Phase 2	Cook Road to SR 28 East Junction	Widening to five lanes with curb and gutter and sidewalks	7.0

Clermont County

PID	Facility	Sec[OH]	Len	Location	Description	Programmed costs (000's)					AQ con- formity	Sponsor	Award/ Let Date	
						Fund Type	Phase	Pre 08	FY 08	FY 09				FY 10
82557	Old SR 74 - 0.00 Phase 1	6.29		Eastgate Road to Bach- Buxton North	Add 1 lane						Total Cost: \$500	Analyzed	Clermont TID	UNSC
						Local	P	0	500	0	0	0	0	0
83661	Stonelick Bridge	0.00	0.00	Stonelick Covered Bridge (TIP Amendment 2-14-08)	Rehabilitate covered bridge						Total Cost: \$450	Exempt	Clermont County	4Q10
						Local	P-E	0	0	8	0	0	0	0
						Fed Disc	P-F	0	0	32	0	0	0	0
						Fed Disc	P-DD	0	0	64	0	0	0	0
						Local	P-DD	0	0	16	0	0	0	0
						Local	C	0	0	0	66	0	0	0
						Fed Disc	C	0	0	0	264	0	0	0
82558	Tina Drive Extension	0.00	0.00	Old SR 74 to Tina Drive	New two-lane connector with turn lanes at Old SR 74 intersection (Admin. Mod. 6-10-08)						Total Cost: \$2,350	Analyzed	Clermont TID	FY12
						TCSP	P	0	0	120	0	0	0	0
						Local	P	0	200	30	0	0	0	0
						Local	R	0	0	0	0	180	0	0
						OKI-STP	R	0	0	0	0	420	0	0
						Local	C	0	0	0	0	0	420	0
						OKI-STP	C	0	0	0	0	0	980	0
86229	Various Routes	0.00	0.00	Various Routes in Clermont County	Resurfacing						Total Cost: \$2,464	Exempt	Clermont County	1Q10
						ARRA	C	0	0	0	2464	0	0	0
82139	Wolfpen-Pleasant Hill	0.00	1.05	From SR 131 to 1.05 miles north of SR 131	Improve Wolfpen-Pleasant Hill by adding center turn lane with a 4-foot sidewalk on one side						Total Cost: \$4,400	Exempt	Clermont County	2Q11
						Local	P	0	400	0	0	0	0	0
						Local	R	0	0	0	250	0	0	0
						OKI-STP	R	0	0	0	250	0	0	0
						OKI-STP	C	0	0	0	0	1750	0	0
						Local	C	0	0	0	0	1750	0	0
80467	East Fork Bikepath	0.00	0.00	Connection between Villages of Williamsburg and Batvia to Eastfork State Park	Hike/bike trail construction (TIP Amendment #13, 1-8-09)						Total Cost: \$516	Exempt	Williamsburg	2Q11
						OKI-ENH	C	0	0	0	0	413	0	0
						Local	C	0	0	0	0	103	0	0
75641	SR 28	2.25	0.00	Bridge No. CLE-28-0225, SFN: 1300016. Bridge carries SR 28 over I-275	Bridge repair and abutment stabilization/correction						Total Cost: \$390	Exempt	ODOT	3Q10
						IM	C	0	0	0	351	0	0	0
						State	C	0	0	0	39	0	0	0
82563	SR 28	0.00	3.13	Branch Hill-Guinea Pike to SR 48	Add 1 lane						Total Cost: \$750	Analyzed	Clermont TID	UNSC
						Local	P	0	750	0	0	0	0	0
82140	SR 28	2.05	0.44	From 0.44 miles west of IR 275 to IR 275 southbound exit ramp	Construct a 5-lane roadway (3 lane existing) with a 4-foot sidewalk on one side						Total Cost: \$3,000	Analyzed	Clermont County	2Q11
						Local	P	0	500	0	0	0	0	0
						OKI-STP	R	0	0	0	400	0	0	0
						Local	R	0	0	0	100	0	0	0
						Local	C	0	0	0	0	400	0	0
						OKI-STP	C	0	0	0	0	1600	0	0
79111	SR 28 Business	0.00	0.00	SR 28 to Cook Road (Admin. Correction 8-6-07)	Widen to 4 through lanes with turn lanes at signalized intersections and landscaped median						Total Cost: \$7,600	Analyzed	Clermont County	4Q11
						Local	P	0	1000	0	0	0	0	0
						OKI-STP	C	0	0	0	0	3000	0	0
						Local	C	0	0	0	0	3600	0	0
75686	SR 32	6.48	28.88	From 0.11 miles west of Batavia corporate line to 0.25 miles west of SR 133	Microsurface a portion of SR 32 in Clermont County						Total Cost: \$770	Exempt	ODOT	1Q12
						State	C	0	0	0	0	0	154	0
						NHS	C	0	0	0	0	0	616	0



**OHIO DEPARTMENT OF TRANSPORTATION
INTEROFFICE COMMUNICATION
Office of Environmental Services**

DATE: December 19, 2008
TO: Brian Mitch, Division of Real Estate and Land Management, ODNR
FROM: Timothy M. Hill, Administrator, Office of Environmental Services
SUBJECT: MOA Project Notifications
PROJECTS: See Attached List

The projects on the attached list will require bridge or culvert replacements and/or roadway improvements on essentially the existing alignments. The specific stream crossings are noted after each project designation and project descriptions, location mapping and photos (if available) are attached for each project. Field surveys were performed for projects where wetland conditions appeared possible and any impact acreages are listed on the attached sheet. These project notifications are in accordance with the Memorandum of Agreement (MOA) for Interagency Coordination for Highway Projects Which Involve Stream Crossings and/or Minor Wetland Fills. Design and construction of the projects will comply with all items of the MOA.

Natural Heritage maps were reviewed for state listed or monitored species within a mile of each of the proposed projects. The attached discussion of counties and species notes the records found and the expected impacts of specific projects on the species.

Your concurrence and/or comments would be appreciated as soon as possible. If comments or notification of when comments will be furnished are not received within 30 days, we will proceed with project development. If you have any questions or concerns, please call Chris Staron, Environmental Specialist, at (614) 466-5112.

TMH:WRC:DER:cs
Enclosures

c: Districts 1, 2, 3, 4, 6, 7, 8, 9, 10, and 11 - File - Reading File



OHIO DEPARTMENT OF TRANSPORTATION

Division of Planning, Office of Environmental Services
1980 West Broad Street, Columbus, Ohio 43223

December 19, 2008

Mary Knapp, Supervisor

U.S. Fish and Wildlife Service
4625 Morse Road, Suite 104
Columbus, OH 43230

Re: **MOA Project Notifications**

Dear Dr. Knapp:

The projects on the attached list will require bridge or culvert replacements and/or roadway improvements on essentially the existing alignments, and other projects that meet a Nationwide 404 Permit. The specific stream crossings are noted after each project designation and project descriptions, location mapping and photos (if available) are attached for each project. Field surveys were performed for projects where wetland conditions appeared possible and any impact acreages are listed on the attached sheet. These project notifications are in accordance with the Memorandum of Agreement (MOA) for Interagency Coordination for Highway Projects Which Involve Stream Crossings and/or Minor Wetland Fills. Design and construction of the projects will comply with all items of the MOA. This coordination effort also satisfies reporting requirements for the Programmatic Indiana bat Biological Opinion, signed January 26, 2007.

The proposed projects are within the known range(s) of federally listed species as noted in the attached discussion of counties, projects and species. These projects are expected to have no more than minimal impacts on federally listed species. Our effects determination for each listed species is included in the attached discussion for each project. However, if a listed or proposed species is subsequently found to occur in the project area, the Federal Highway Administration will initiate coordination with the U.S. Fish and Wildlife Service pursuant to Section 7 of the Endangered Species Act of 1973, as amended.

Your concurrence and/or comments would be appreciated as soon as possible. If comments or notification of when comments will be furnished are not received within 30 days, we will proceed with project development. If you have any questions or concerns, please call Chris Staron, Environmental Specialist, at (614) 466-5112.

Sincerely,

Timothy M. Hill
Administrator
Office of Environmental Services

TMH:WRC:DER:cs
Enclosures

c: Districts 1, 2, 3, 4, 6, 7, 8, 9, 10, and 11 - File - Reading File

December 19, 2008

MOA Project Notifications

DISTRICT	C-R-S	PID	WATERBODY	WETLANDS
1	HAN-CR 95-0.00	75797	None	None
2	FUL-US 20/ SR 109-13.76/5.80	80672	Bad Creek, Bear Creek	None
3	ERI-Cedar Point Drive	82670	3 Un. Tribs. to Pipe Creek	<0.1 acre
3	RIC-C.R. 175-4.62	84043	Trib. of Paramour Creek	None
4	ATB-Lake Ave. Bridge	80653	None	None
6	UNI-TR 99-0.24	82356	Mill Creek	None
7	CLA-70-13.98	84664	3 Un. Tribs. to Buck Creek	None
7	MER-TR 61-25.41	83314	St. Mary's River	0.14 acre
8	CLE-28-2.05	82140	Trib to Little Miami River	None
8	HAM-SR 4-7.44	83080	None	None
9	JAC-SR 93-2.94	23022	Hunting Camp Creek	None
9	LAW-US 52-4.24	20429	Osborne Run	None
10	ATH-681-5.91	77636	Un. Trib. to Lake Snowden	None
10	NOB-83-4.19	79562	Rannells Creek, West Fork Duck Creek	None
10	NOB-147-6.10	79547	Little Buffalo Creek, Opossum Run, Un. Trib. to Opossum Run	None
10	NOB-566-1.19	82998	Opossum Run, Un. Trib. to Opossum Run	None
10	VIN-Zaleski S.F. FY 2010	84873	5 Un. Tribs. to Raccoon Creek	None
10	VIN-327-12.53	75658	Little Sandy Run	None
10	WAS-7-20.31	25092	Mile Run, Un. Trib. to West Fork Duck Creek	None
11	HOL-179-0.67	24944	3 Un. Tribs. to Crab Run	None

Projects listed in bold are projects that May Affect a federally listed species and require full USFWS review.

LISTED SPECIES

1. Federally Listed Species who's Known Ranges Include the Counties of the Projects:

ALL COUNTIES -

Indiana Bat

All of the projects are within the known range of the federally endangered Indiana bat (*Myotis sodalis*). The impacts of each project on suitable habitat for the species are noted below. On each of the projects, tree-cutting will be minimized. The Programmatic Agreement will be followed. Unless USFWS has other recommendations, for projects that require the use of cutting dates, unavoidable cutting of any trees which may provide suitable roosting or brood-rearing habitat for the species (i.e. living or standing dead trees or snags with exfoliating, peeling or loose bark, split trunks and/or branches, or cavities) will be performed only before April 15 or after September 15 when the species would not be using such habitat. This will minimize the chance that any of the projects will impact this species.

Bald Eagle

This species was recently removed from the Endangered Species list, but remains protected under the Bald and Golden Eagle Protection Act. ODOT will continue to coordinate for this species in the same way that it has been coordinated in the past to satisfy coordination under the Bald and Golden Eagle Protection Act. Effect calls will still be generated for this species.

HANCOCK COUNTY

HAN-CR 95-0.00 (PID: 75797) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**), the Bald Eagle (*Haliaeetus leucocephalus*, **T**), the Clubshell Mussel (*Pleurobema clava*, **E**), and the Rayed Bean (*Villosa fabalis*, **C**). This project proposes to widen and resurface the existing pavement along with install new storm water drainage. Based on the project type, location, and on-site habitat, none of these species, except the Indiana bat, would be expected within the project area, and no impacts to these species are expected.

Indiana bat – The project area is located within the western management unit. A survey of the entire project study area did not identify any areas that would be considered suitable winter hibernacula habitat for the bat, and the nearest identified capture records and suspected hibernacula within Ohio are located more than 10 miles from the project area. Construction activities associated with the project would result in impacts to eight (4) trees with potentially suitable summer roosting habitat and no trees with potentially

suitable maternity roosting habitat. In addition, the impacts will occur in forested areas less than 100 acres in size. Due to the project type, location, and onsite habitat, the project **May Affect, but is Not Likely to Adversely Affect** the Indiana bat (PC1-b).

FULTON COUNTY

FUL-US 20/SR 109-13.76/5.80 (PID: 80672) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**) and the Eastern Massasauga (*Sistrurus catenatus*, **C**). This project proposes to resurface a two-lane road along with repairing two bridges over small perennial streams. Based on the project type, location, and on-site habitat, none of these species would be expected within the project area, and no impacts to these species are expected.

ERIE COUNTY

ERI-Cedar Point Drive (PID: 82670) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**), the Bald Eagle (*Haliaeetus leucocephalus*, **T**), the Piping Plover (*Charadrius melodus*, **E**), the Eastern Massasauga (*Sistrurus catenatus*, **C**), the Lake Erie Watersnake (*Nerodia sipedon insularum*, **T**), and the Lakeside Daisy (*Hymenoxys herbacea*, **T**). This project proposes to resurface Cedar Point Drive along with construct sidewalks and curb ramps. There are no known Bald Eagle nests within a half mile of the project area. There are two small wetlands adjacent to Pipe Creek within the project area but they do not provide suitable habitat for the Eastern Massasauga since they are degraded. Based on the project type, location, and on-site habitat, none of these species would be expected within the project area, and no impacts to these species are expected.

RICHLAND COUNTY

RIC-C.R. 175-4.62 (PID: 84043) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**) and the Bald Eagle (*Haliaeetus leucocephalus*, **T**). This project proposes to replace a bridge over an intermittent stream that runs through agricultural fields. No trees will be removed for this project. Based on the project type, location, and on-site habitat, none of these species would be expected within the project area, and no impacts to these species are expected.

ASHTABULA COUNTY

ATB-Lake Ave. Bridge (PID: 80653) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**), the Bald Eagle (*Haliaeetus leucocephalus*, **T**), the Piping Plover (*Charadrius melodus*, **E**), the Eastern Massasauga (*Sistrurus catenatus*, **C**), and the Clubshell Mussel (*Pleurobema clava*, **E**). This project proposes to replace a bridge over the Norfolk Southern railroad along with make improvements to several intersections in the city of Ashtabula. This project is within 5 miles of a summer capture site, but no suitable Indiana bat habitat will be removed for this project. Based on the project type,

location, and on-site habitat, none of these species would be expected within the project area, and no impacts to these species are expected.

UNION COUNTY

UNI-TR99-0.24 (PID: 82356) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**), the Scioto Madtom (*Noturus trautmanii*, **E**), the Northern Riffleshell (*Epioblasma torulosa rangiana*, **E**), Clubshell Mussel (*Pleurobema clava*, **E**), and the Rayed Bean (*Villosa fabalis*, **C**). This project proposes to replace a bridge over a perennial stream. Based on the project type, location, and on-site habitat, none of these species, except the Indiana bat, would be expected within the project area, and no impacts to these species are expected.

Indiana Bat – Three trees to be removed contain habitat that is suitable for the Indiana Bat. These trees do not offer suitable maternity roost habitat. However, these trees are connected to a 100 acre woodlot, and are directly adjacent to a perennial waterway. No records of bat captures are found within 5 miles of the project area, and no potential hibernacula are found within 10 miles of the project area. The project falls under PC 2-a, and will require the use of cutting dates. The revised cutting dates of April 1 to September 30 will be used if possible. This project **May Affect, but is Not Likely to Adversely Affect** this species.

CLARK COUNTY

CLA-70-13.98 (PID 84664) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**), the Eastern Massasauga (*Sistrurus catenatus*, **C**), and the Eastern Prairie Fringed Orchid (*Platanthera leucophaea*, **T**). The proposed project involves the widening of IR 70 for approximately seven (7) miles. It is anticipated that all road widening work will occur within the interior median of the existing IR 70. As all bridges within the project area have already been replaced to accommodate the roadway widening (previously coordinated), in-stream work activities will be limited to connecting new drainage basins to the existing culverts and replacing “in-kind” any deficient culverts identified during construction. Any culvert extensions necessary to accommodate outside shoulder work will be limited to a few feet on each side of the culvert. Based on the project type, location, and on-site habitat, none of these species, except the Indiana bat, would be expected within the project area, and no impacts to these species are expected.

Indiana Bat – The project area is located within the western management unit as defined by the *Biological Opinion on the Ohio Department of Transportation's Statewide Transportation Program for the Federally-listed endangered Indiana bat (Myotis sodalis)*, dated January 26, 2007. A survey of the entire project study area did not identify any areas that would be considered suitable winter hibernacula habitat for the bat, and the nearest identified capture record and suspected hibernacula within Ohio are located more than 10 miles from the project area. Construction activities associated with the project would result in impacts to five (5) trees with potentially suitable summer

roosting habitat and no (0) trees with potentially suitable maternity roosting habitat. In addition, the impacts will occur in forested areas less than 100 acres in size (PC1-b). Based on the above discussion, this project **May Affect, but is Not Likely to Adversely Affect** this species.

MERCER COUNTY

MER-TR 61-25.41 (PID: 83314) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, E) and the Bald Eagle (*Haliaeetus leucocephalus*, T). This project proposes to replace a bridge over a perennial stream. No trees over 8" dbh will be removed for this project. Based on the project type, location, and on-site habitat, none of these species would be expected within the project area, and no impacts to these species are expected.

CLERMONT COUNTY

CLE-28-2.05 (PID: 82140) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, E), the Rayed Bean (*Villosa fabalis*, C), the Sheepnose Mussel (*Plethobasus cyphus*, C), and Running Buffalo Clover (*Trifolium stoloniferum*, E). This project proposes to widen SR 28 to a uniform five lanes from Castleberry Court to IR-275. No trees exhibiting suitable roosting habitat will be removed for this project. The small stream being impacted by this project does not offer suitable mussel habitat. Based on the project type, location, and on-site habitat, none of these species, except the Running Buffalo Clover, would be expected within the project area, and no impacts to these species are expected.

Running Buffalo Clover – One small area near Stream 1 in the central portion of the study area offers potentially suitable habitat for this species. Biologists from LJB Inc. surveyed this parcel on May 19, 2008 for this species, and no individuals were found. Based on the above discussion, this project **May Affect, but is Not Likely to Adversely Affect** this species.

HAMILTON COUNTY

HAM-SR 4-7.44 (PID: 83080) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, E), the Sheepnose Mussel (*Plethobasus cyphus*, C), and Running Buffalo Clover (*Trifolium stoloniferum*, E). This project is a safety project that proposes to upgrade existing traffic signals, trim trees, and re-configure the northern driveway from the Goodwill Industries property. John Baird, ODOT-OES conducted a field review on May 15, 2008 to determine if running buffalo clover populations or habitat was found within the project area. Based on his field review, no suitable habitat for this species will be impacted by this project. This project is also located entirely within the Indiana Bat urbanized area. Based on the project type, location, and on-site habitat, none of these species would be expected within the project area, and no impacts to these species are expected.

JACKSON COUNTY

JAC-SR 93-2.94 (PID: 23022) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**) and the Timber Rattlesnake (*Crotalus horridus horridus*, **PC**). This project proposes to replace a culvert over a small perennial stream in a residential area. Based on the project type, location, and on-site habitat, the Timber Rattlesnake would not be expected within the project area, and no impacts to this species are expected.

Indiana Bat – Two trees to be removed contain habitat that is suitable for the Indiana Bat. These trees do not offer suitable maternity roost habitat for this species. No records of bat captures are found within 5 miles of the project area, and no potential hibernacula are found within 10 miles of the project area. The project falls under PC1-a, and **May Affect, but is Not Likely to Adversely Affect** this species.

LAWRENCE COUNTY

LAW-US 52-4.24 (PID: 20429) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**), the Timber Rattlesnake (*Crotalus horridus horridus*, **PC**), the Sheepnose Mussel (*Plethobasus cyphus*, **C**), the Pink Mucket (*Lampsilis abrupta*, **E**), and Running Buffalo Clover (*Trifolium stoloniferum*, **E**). This project proposes to line an existing steel culvert on a small perennial stream within ½ mile of the Ohio River. Based on the photos of the project area, John Baird, ODOT Environmental Specialist, stated that no suitable habitat for the Running Buffalo Clover is found within the project area. This project is also within 10 miles of a potential hibernacula site, but no suitable Indiana bat habitat will be removed for this project. The riparian woods adjacent to the project area floods often from the Ohio River and would not offer suitable habitat for the Timber Rattlesnake. The mussel species could be found in the Ohio River near the project area, but the culvert lining will not produce enough siltation impacts to potentially affect the Ohio River and its fauna. Due to the project type, location, and onsite habitat, none of these species

ATHENS COUNTY

ATH-681-5.91 (PID: 77636) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**), the Timber Rattlesnake (*Crotalus horridus horridus*, **PC**), the Sheepnose Mussel (*Plethobasus cyphus*, **C**), and the American Burying Beetle (*Nicrophorus americanus*, **E**). This project proposes to replace a culvert over a small perennial stream. Due to the project type, location, and onsite habitat, none of these species would be expected within the project area, and no impacts to these species are expected.

NOBLE COUNTY

NOB-83-4.19 (PID: 79562) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**) and the Bald Eagle (*Haliaeetus leucocephalus*, **T**). This project

proposes to replace two bridges over an intermittent and small perennial stream. Due to the project type, location, and onsite habitat, none of these species would be expected within the project area, and no impacts to these species are expected.

NOB-147-6.10 (PID: 79547) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**) and the Bald Eagle (*Haliaeetus leucocephalus*, **T**). This project proposes to replace three structures over two intermittent and one small perennial stream. Due to the project type, location, and onsite habitat, none of these species would be expected within the project area, and no impacts to these species are expected.

NOB-566-1.19 (PID: 82998) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**) and the Bald Eagle (*Haliaeetus leucocephalus*, **T**). This project proposes to replace two culverts over an intermittent and small perennial stream. Based on the project type, location, and on-site habitat, the Bald Eagle would not be expected within the project area, and no impacts to this species are expected.

Indiana Bat – One tree to be removed contains habitat that is suitable for the Indiana Bat. This tree does not offer suitable maternity roost habitat for this species. No records of bat captures are found within 5 miles of the project area, and no potential hibernacula are found within 10 miles of the project area. The project falls under PC1-a, and **May Affect, but is Not Likely to Adversely Affect** this species.

VINTON COUNTY

VIN-Zaleski S.F. FY 2010 (PID: 84873) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**), the Timber Rattlesnake (*Crotalus horridus horridus*, **PC**), and the American Burying Beetle (*Nicrophorus americanus*, **E**). This project proposes to replace or rehabilitate five structures over ephemeral and intermittent streams. Wetland habitats are located at two of these locations. These habitats are palustrine emergent marshes dominated by wool-rush. These habitats will not be disturbed during this maintenance operation. Due to the project type, location, and onsite habitat, none of these species would be expected within the project area, and no impacts to these species are expected.

VIN-327-12.53 (PID: 75658) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**), the Timber Rattlesnake (*Crotalus horridus horridus*, **PC**), and the American Burying Beetle (*Nicrophorus americanus*, **E**). This project was previously coordinated on the September 30, 2008 MOA. Since then, an additional bridge has been added to the scope of the project. The bridge proposes to be replaced over an intermittent stream. This project is within 5 miles of a summer capture site, but no suitable Indiana bat habitat will be removed for this project. Due to the project type, location, and onsite habitat, none of these species would be expected within the project area, and no impacts to these species are expected.

WASHINGTON COUNTY

WAS-7-20.31 (PID: 25092) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**), the Fanshell Mussel (*Cyprogenia stegaria*, **E**), the Sheepnose Mussel (*Plethobasus cyphus*, **C**), and the Pink Mucket (*Lampsilis abrupta*, **E**). This project proposes to install a spiral liner into two existing culverts over an intermittent and small perennial stream. Due to the project type, location, and onsite habitat, none of these species would be expected within the project area, and no impacts to these species are expected.

Sheepnose Mussel – This species is recorded from the Ohio River adjacent to the project area. The two streams that will be impacted by this project do not offer suitable habitat for this species. The lining of the culverts will require very little land clearing and stream bottom disturbance. Any siltation caused by this project would be very minimal and of very limited duration. The siltation should not increase the background siltation that the Ohio River experiences during rain events. Based on the above discussion, this project **May Affect, but Will Not Likely Adversely Affect** this species.

HOLMES COUNTY

HOL-179-0.67 (PID: 24944) -- This project is within the known range of the Indiana Bat (*Myotis sodalis*, **E**), the Bald Eagle (*Haliaeetus leucocephalus*, **T**), and the Eastern Prairie Fringed Orchid (*Platanthera leucophaea*, **T**). This project proposes to replace three culverts over intermittent streams. Based on the project type, location, and on-site habitat, none of these species, except the Indiana bat, would be expected within the project area, and no impacts to these species are expected.

Indiana Bat – One tree to be removed contain habitat that is suitable for the Indiana Bat. This tree does not offer suitable maternity roost habitat for this species. No records of bat captures are found within 5 miles of the project area, and no potential hibernacula are found within 10 miles of the project area. The project falls under PC1-a, and **May Affect, but is Not Likely to Adversely Affect** this species.

2: State Listed or Monitored Species - Results of Review of Natural Heritage Maps:

ERI-Cedar Point Drive (PID: 82670) -- Numerous records are found within one mile of the project area. These records are listed below:

1. Common Tern (*Sterna hirundo*, **E**)
2. Threehorn Wartyback (*Obliquaria reflexa*, **T**)
3. Deertoe (*Truncilla truncate*, **SC**)
4. Fawnsfoot (*Truncilla donaciformis*, **T**)
5. Muskellunge (*Esox masquinongy*, **SC**)

These species are recorded from Sandusky Bay and the Pipe Creek Wildlife Area. The area that will be impacted by this project contains business properties, mowed right-of-way, and small amounts of wetland edge along the Pipe Creek estuary area. These wetlands are dominated by Giant Reed (*Phragmites australis*), an invasive species that does not offer any suitable nesting habitat for the common tern. The Pipe Creek estuary will not be impacted; therefore no impacts to the mussel species or the muskellunge are expected.

MER-TR 61-25.41 (PID: 83314) – A record for Pigeon Grape (*Vitis cinerea*, **P**) and Lake Cress (*Armoracia lacustris*, **T**) was found within 1/4 mile of the project area in the adjacent riparian woods. This project will only impact ODOT's right-of-way. Most of the ROW is mowed, but some does contain young trees and herbaceous vegetation. That may provide some suitable habitat for these species. These species have not been recorded from the project area, but no vegetation surveys were performed within the project area.

HAM-SR 4-7.44 (PID: 83080) – A record for Deam's Three-seeded Mercury (*Acalypha virginica* var. *deamii*, **P**) was found within a mile of the project area. The habitat directly adjacent to the project area consists of mowed lawn and right-of-way. No suitable habitat for this species is found within this project area. No impacts to this species are expected.

JAC-SR 93-2.94 (PID: 23022) – A record for the Lake Chubsucker (*Erimyzon sucetta*, **T**) was found within one mile of the project area in Black Fork Symmes Creek. This species is generally found in lakes and the deeper segments of slow-moving large streams with clear water, gravel bottoms, and abundant aquatic vegetation. The stream that will be impacted by this project is a very small perennial waterway with gravel and sand substrates, no deeper pools, and no aquatic vegetation. No suitable habitat for this species is found within the project area; therefore no impacts to this species are expected.

LAW-US 52-4.24 (PID: 20429) – Records for Small-flowered Alum-root (*Heuchera parviflora*, **T**), Spanish Oak (*Quercus falcate*, **T**), and Virginia-mallow (*Sida hermaphrodita*, **P**) were found within one mile of the project area. This project proposes to line the existing culvert and will only disturb the roadside vegetation. The photos for this project are somewhat old, and most of the woody vegetation has been removed from the roadway slopes. The small-flowered alum root is found on shaded sandstone exposures, the Spanish oak is found in dry upland forests, and the Virginia-mallow is

found in disturbed sandy areas along rivers, generally along the Ohio River in Ohio. No suitable habitats for any of these species are found within this project area. No impacts to these species are expected.

VIN-Zaleski S.F. FY 2010 (PID: 84873) -- Numerous plant records are found within one mile of this project. These plants are listed below:

1. Netted Chain-fern (*Woodwardia virginica*, **N**)
2. Large Marsh St. John's-wort (*Triadenum tubulosum*, **T**)
3. Green Adder's-mouth (*Malaxis unifolia*, **P**)
4. Reflexed Sedge (*Carex retroflexa*, **T**)

No suitable habitats for any of these species are found within this project area. No impacts to these species are expected.

VIN-327-12.53 (PID: 75658) -- A record for Large Marsh St. John's-wort (*Triadenum tubulosum*, **T**), Green Adder's-mouth (*Malaxis unifolia*, **P**), and Tubercled Rein Orchid (*Plantanthera flava*, **P**) was found within one mile of the project area. No suitable habitats for any of these species are found within this project area. No impacts to these species are expected.

WAS-7-20.31 (PID: 25092) -- Numerous records are found within one mile of the culvert at WAS-7-20.31. These records are listed below:

1. Ohio Lamprey (*Ichthyomyzon bdellium*, **E**)
2. Black Sandshell (*Ligumia recta*, **T**)
3. Sheepnose (*Plethobasus cyphyus*, **E**)
4. Threehorn Wartyback (*Obliquaria reflexa*, **T**)
5. Fawnsfoot (*Truncilla donaciformis*, **T**)

No suitable habitats for any of these species are found within this project area. No impacts to these species are expected.



Mark Clark/Planning/D08/ODOT

01/30/2009 02:10 PM

To Scott Brown/Planning/D08/ODOT@ODOT, Katie DeStefano/Planning/D08/ODOT@ODOT, Greg Patton/Planning/D08/ODOT@ODOT, Nicholas

cc

bcc

Subject Fw: 09-0004; ODOT MOA Dated December 19, 2008

Mark E. Clark, P.E., District Planning Dept. Team Leader
Ohio Dept. of Transportation, District 8
Phone: 1-800-831-2142, ext. 933-6595 or 513-933-6595
Fax 513-932-9366

Internet e-mail address: mark.clark@dot.state.oh.us

----- Forwarded by Mark Clark/Planning/D08/ODOT on 01/30/2009 02:08 PM -----



Chris Staron/Environmental /CEN/ODOT

01/30/2009 02:03 PM

To Clark Nash/Planning/D01/ODOT@ODOT, Richard Perse/Planning/D02/ODOT@ODOT, Mac Vance/Planning/D03/ODOT@ODOT, Kris Rickett/Planning/D03/ODOT@ODOT, Edward Deley/Planning/D04/ODOT@ODOT, Melissa Nemeth/Planning/D04/ODOT@ODOT, Brian Peck/Planning/D04/ODOT@ODOT, Brian Tatman/Planning/D06/ODOT@ODOT, Janice Gartner/Planning/D06/ODOT@ODOT, John Horman/Planning/D07/ODOT@ODOT, Tricia Bishop/Planning/D07/ODOT@ODOT, Keith Smith/Planning/D08/ODOT@ODOT, Nicholas Smith/Planning/D08/ODOT@ODOT, Mark Clark/Planning/D08/ODOT@ODOT, Greg Manson/Planning/D09/ODOT@ODOT, Delvin Murray/Planning/D09/ODOT@ODOT, Michael Austin/Planning/D10/ODOT@ODOT, Thomas Stratton/Planning/D11/ODOT@ODOT, Amber Hewitt/Planning/D11/ODOT@ODOT

cc Donald Rostofer/Environmental/CEN/ODOT@ODOT, Megan Michael/Environmental/CEN/ODOT@ODOT

Subject Fw: 09-0004; ODOT MOA Dated December 19, 2008

All, below are ODNRs comments on the December 2008 MOA.

FUL-US 20/SR 109-13.75/5.80 - No listed mussels or mussel populations were found in the DNAP database. This office believes the streams are too small to support native populations of mussels , thus OES will not perform any additional field survey for mussels .

CLA-70-13.98 - has crossings on 4 streams that eventually empty into the N. Fk. Little Miami River. This project is over 5 miles from the LMR itself. The streams that are in the LMR watershed probably will not be impacted because the bridges have already been replaced over these streams . The streams that have anticipated impacts flow to Buck Creek in the Mad River drainage . The comments from ODNr are recommendations only.

CLE-28-2.05 - is a direct tributary to LMR, but is over 1000 feet from the river (its about 1 mile from LMR). These comments should also be treated as recommendations only.

UNI-TR 99-0.24 - This project should be sampled for mussels by ODOT , OES. It was sent in too late in

the year for a survey. OES will survey the site in the spring at the first opportunity .

LAW-52-4.24 - ODNR believes the project will not impact the salamander or the spadefoot toad , but if either is encountered during construction, the construction must stop and DOW must be notified. This commitment must be added as a plan note.

ATH-681-5.91 - ODNR believes the project will not impact the spadefoot toad , but if the species is encountered during construction, the construction must stop and DOW must be notified. This commitment must be added as a plan note.

VIN-Zalenski S.F. - ODNR believes the project will not impact the timber rattlesnake , but if the species is encountered during construction, the construction must stop and DOW must be notified. This commitment must be added as a plan note.

WAS-7-20.31 - ODNR believes the project will not impact the spadefoot toad , but if the species is encountered during construction, the construction must stop and DOW must be notified. This commitment must be added as a plan note.

If you have any further questions, please contact me.

Good day

Chris

----- Forwarded by Chris Staron/Environmental/CEN/ODOT on 01/30/2009 01:26 PM -----



"Mitch, Brian"
<Brian.Mitch@dnr.state.oh.us>
01/29/2009 01:32 PM

To <tim.hill@dot.state.oh.us>
cc <Donald.Rostofer@dot.state.oh.us>, <Chris.Staron@dot.state.oh.us>
Subject 09-0004; ODOT MOA Dated December 19, 2008



ODNR COMMENTS TO Timothy M. Hill, Administrator, ODOT Office of Environmental Services, 1980 West Broad Street, Columbus, Ohio 43223

Location: The ODOT MOA Dated December 19th, 2008 involves PIDs 75797, 80672, 82760, 84043, 80653, 82356, 84664, 83314, 82140, 83080, 23022, 20429, 77636, 79562, 79547, 82998, 84873, 75658, 25092 and 24944. These projects are located in ODOT Districts 1, 2, 3, 4, 6, 7, 8, 9, 10 and 11.

Project: These projects will require bridge or culvert replacements and/or road way improvements on essentially the existing alignments.

The Ohio Department of Natural Resources (ODNR) has completed a review of the above referenced project. These comments were generated by an inter-disciplinary review within the Department. These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (48 Stat. 401, as amended; 16 U.S.C. 661 et seq.), the National Environmental Policy Act, the Coastal Zone Management Act, Ohio Revised Code and other applicable laws and regulations. These comments are also based on ODNR's experience as the state natural resource management agency and do not supersede or replace the regulatory authority of any local, state or federal agency nor relieve the applicant of the obligation to comply with any local, state or federal laws or regulations.

Rare and Endangered Species: The ODNR, Division of Natural Areas and Preserves, has comments on the following projects:

84664 - This project crosses tributaries to the North Fork Little Miami State and National Scenic River. Scenic Rivers provides the following recommendations.

1. A sediment and erosion control plan should be developed for the site and implemented before earthwork commences. Particular attention should be given to any drainage ways, ditches and streams that could convey sediment laden water directly to the unnamed tributaries of the North Fork Little Miami. Properly installed (framed and entrenched) sediment fence should be utilized around the work site perimeter and storm water inlets. Appropriately designed rock-check dams and other erosion controls should be utilized in ditches and drainage ways. All controls should be properly maintained until final site stabilization is achieved. All sediment and erosion controls should be removed upon stabilization of the project area with vegetation. Straw bales should not be permitted as a form of erosion control. All denuded areas, including ditches, culverts and river/stream banks, should be permanently seeded and mulched (or fiber mat) immediately upon completion of earthwork or temporarily seeded and mulched (or fiber mat) within seven days if the area is to remain idle for more than thirty days. Access roads constructed on slopes shall be graveled to prevent erosion from surface runoff.

2. Idle equipment, petrochemicals and toxic/hazardous materials should not be stored in the floodplain or near any drainage ways, ditches or streams that could convey such materials to the unnamed tributaries of the North Fork Little Miami. Petrochemicals and toxic/hazardous materials should not be discharged into the unnamed tributaries of the North Fork Little Miami, its floodplain or any drainage ways, ditches or streams. Refueling of equipment shall not occur in the floodplain or near any drainage ways, ditches or streams. A spill containment and cleanup plan should be generated prior to the start of the project.

3. All in-stream or stream crossing work should be conducted during low flow period (August 1 through October 31). Any disturbed areas in the stream beds should be returned to pre-construction contours. Stream bottom elevations

should be determined before in-stream work commences to ensure that all fill material and debris is completely removed before construction is completed. Rip- rap used should be kept to the minimum amount needed to prevent scour and shall consist of clean rock only (free of any toxic or fine material). All fill material used as rip rap, work platforms or cofferdams should be a minimum of three inches in diameter and be washed to remove fine particulate matter (clay, silt, sand and soil). Work platforms should be kept to the absolute minimum size needed to facilitate in-stream work. In-stream work should be conducted through the use of water diversions not requiring the placement of earthen fill (sheet piling, membrane dams, etc.) wherever possible. Any fill should be completely removed from the streambed immediately upon completion of in-stream work.

4. If dewatering becomes necessary throughout any portion of the project, all wastewater should be pumped onto a vegetated area a sufficient distance from the unnamed tributaries of the North Fork Little Miami to allow for complete infiltration. No wastewater of any kind should be discharged directly into the unnamed tributaries of the North Fork Little Miami or any other drainage ways, ditches or streams. All storm water drainage should be directed onto a vegetated area to allow for complete infiltration. If discharge to a vegetated area is not feasible, then wastewater should be discharged into a sediment filter bag or into a temporary detention/retention pond with sufficient retention time to permit for the settling of all suspended solids.

5. All streambank vegetation should be left undisturbed to the maximum extent possible. Areas where vegetation is removed should be re-vegetated with native tree species. Any disturbed streambanks should be returned to previously existing contours and elevations. Trees should be one inch in diameter and balled/ burlap nursery stock. After a full growing season for the trees, any stakes and guide wires should be removed and properly disposed of. Any trees that die during the first growing season should be replaced. Cutting or clearing of any riparian vegetation within 1000 feet of the unnamed tributaries of the North Fork Little Miami beyond the existing right-of-way should be prohibited, however vertical trimming is permitted where necessary. Care should be taken not to girdle or scuff tree trunks or damage any standing trees.

If there are any questions on these recommendations, please contact Scenic Rivers Group Manager Bob Gable at 614-265-6814.

→ 82140 - This project is within the Little Miami State and National Scenic River watershed, and within one mile of the Little Miami. Scenic Rivers provides the following recommendations.

1. A sediment and erosion control plan should be developed for the site and implemented before earthwork commences. Particular attention should be given to any drainage ways, ditches and streams that could convey sediment laden water directly to the unnamed tributary of the Little Miami. Properly installed (framed and entrenched) sediment fence should be utilized around the work site perimeter and storm water inlets. Appropriately designed rock-check dams and other erosion controls should be utilized in ditches and drainage ways. All controls should be properly maintained until final site stabilization is achieved. All sediment and erosion controls should be removed upon stabilization of the project area with vegetation. Straw bales should not be permitted as a form of erosion control. All denuded areas, including ditches, culverts and river/stream banks, should be permanently seeded and mulched (or fiber mat) immediately upon completion of earthwork or temporarily seeded and mulched (or fiber mat) within seven days if the area is to remain idle for more than thirty days. Access roads constructed on slopes shall be graveled to prevent erosion from surface runoff.

0.350 miles

2. Idle equipment, petrochemicals and toxic/hazardous materials should not be stored in the floodplain or near any drainage ways, ditches or streams that could convey such materials to the unnamed tributary of the Little Miami. Petrochemicals and toxic/hazardous materials should not be discharged into the unnamed tributary of the Little Miami, its floodplain or any drainage ways, ditches or streams. Refueling of equipment shall not occur in the floodplain or near any drainage ways, ditches or streams. A spill containment and cleanup plan should be generated prior to the start of the project.

SS832, Section M and SS832-OEPA Permit No. OHC000002, Part V.G.2(I) &(M) prohibit this action. Can be managed via plan note and/or SS832 on plan title sheet.

3. All in-stream or stream crossing work should be conducted during low flow period (August 1 through October 31). Any disturbed areas in the stream beds should be returned to pre-construction contours. Stream bottom elevations should be determined before in-stream work commences to ensure that all fill material and debris is completely removed before construction is completed. Rip- rap used should be kept to the minimum amount needed to prevent scour and shall consist of clean rock only (free of any toxic or fine material). All fill material used as rip rap, work platforms or cofferdams should be a minimum of three inches in diameter and be washed to remove fine particulate matter (clay, silt, sand and soil). Work platforms should be kept to the absolute minimum size needed to facilitate in-stream work. In-stream work should be conducted through the use of water diversions not requiring the placement of earthen fill (sheet piling, membrane dams, etc.) wherever possible. Any fill should be completely removed from the streambed immediately upon completion of in-stream work.

Can be managed via a plan note. Remove non-applicable language. With only one jurisdictional stream (24" culvert), this work can be performed within the given time frame.

4. If dewatering becomes necessary throughout any portion of the project, all wastewater should be pumped onto a vegetated area a sufficient distance from the unnamed tributary of the Little Miami to allow for complete infiltration. No wastewater of any kind should be discharged directly into the unnamed tributary of the Little Miami or any other drainage ways, ditches or streams. All storm water drainage should be directed onto a vegetated area to allow for complete infiltration. If discharge to a vegetated area is not feasible, then wastewater should be discharged into a sediment filter bag or into a temporary detention/retention pond with sufficient retention time to permit for the settling of all suspended solids.

First sentence is non-applicable. Second sentence handled under SS832 & the NPDES. Third sentence is managed naturally by virtue of the storm water being directed into/towards an empty field. Can be managed via a plan note. Remove non-applicable language.

5. All streambank vegetation should be left undisturbed to the maximum extent possible. Areas where vegetation is removed should be re-vegetated with native tree species. Any disturbed streambanks should be returned to previously existing contours and elevations. Trees should be one inch in diameter and balled/ burlap nursery stock. After a full growing season for the trees, any stakes and guide wires should be removed and properly disposed of. Any trees that die during the first growing season should be replaced. Cutting or clearing of any riparian vegetation within 1000 feet of the unnamed tributary of the Little Miami beyond the existing right-of-way should be prohibited, however vertical trimming is permitted where necessary. Care should be taken not to girdle or scuff tree trunks or damage any standing trees.

All trees removed shall be replaced in-kind. Provide a quantity and locate on the plans. Can be managed via a plan note.

If there are any questions on these recommendations, please contact Scenic Rivers Group Manager Bob Gable at 614-265-6814.

83080 - This project borders Trillium Trails of the Hamilton Co. Park District. The park district should also be consulted on this review.

Our inventory program has not completely surveyed Ohio and relies on information supplied by many individuals

and organizations. Therefore, a lack of records for any particular area is not a statement that rare species or unique features are absent from that area.

Fish and Wildlife: The ODNR, Division of Wildlife (DOW) has the following comments.

The DOW recommends comments regarding the Indiana bat are revised to indicate unavoidable cutting of any trees which may provide suitable habitat for the species will be performed only before **April 1** or after **September 30**.

FUL-US 20/SR 109-13.76/5.80 (PID 80672) and UNI-TR 99-0.24 (PID 82356):

These projects must not have an impact on freshwater mussels in the area. This applies to both listed and non-listed species. Surveys and other information regarding the location of mussels in Ohio are limited. This includes information regarding the possibility of mussels being located in the area of these projects. Therefore, the possibility of mussels within the area of the projects cannot be eliminated. If ODOT encounters mussels during construction of the projects, work must stop until a mussel survey is done by a professional malacologist.

LAW-US 52-4.24 (PID 20429):

The project is near known records for the green salamander (*Aneides aeneus*), a state endangered amphibian, and the Eastern spadefoot toad (*Scaphiopus holbrookii*), a state endangered species. The Eastern spadefoot toad is found in areas of sandy soils that are associated with river valleys. Breeding habitats may include flooded agricultural fields or other water holding depressions. Since work for this project is limited to lining an existing culvert, the DOW believes the project is not likely to impact these species. However, ODOT should be made aware of the project's close proximity to known sites for these species. If either species is encountered during construction of the project, work should immediately be stopped, and the DOW should be contacted.

ATH-681-5.91 (PID 77636):

Known records for the Eastern spadefoot toad (*Scaphiopus holbrookii*), a state endangered species, are located in the Hocking River floodplain east of the project. The Eastern spadefoot toad is found in areas of sandy soils that are associated with river valleys. Breeding habitats may include flooded agricultural fields or other water holding depressions. If this species is encountered during construction of the project, work should immediately be stopped and the DOW should be contacted.

VIN-Zaleski S.F. FY 2010 (PID 84873) and VIN-327-12.53 (PID 75658):

These projects are located near known records for the timber rattlesnake (*Crotalus horridus horridus*), a state endangered species. Since work for these projects is limited to simple maintenance within the maintained rights-of-way with no new terrestrial or aquatic impacts, the DOW believes the projects are not likely to impact this species. However, ODOT should be made aware of the projects' close proximity to known sites for this species. If a timber rattlesnake is encountered during construction of the projects, work should immediately be stopped, and the DOW should be contacted.

WAS 7-20.31 (PID 25092):

Known records for the Eastern spadefoot toad (*Scaphiopus holbrookii*), a state endangered species, are located in the Muskingum River floodplain northwest of this project. The Eastern spadefoot toad is found in areas of sandy soils that are associated with river valleys. Breeding habitats may include flooded agricultural fields or other water holding depressions. Since the project is limited to lining an existing culvert, the DOW believes the project is not likely to impact this species. However, if this species is encountered during construction of the project, work should immediately be stopped and the DOW should be contacted.

Parks and Recreation: The ODNR, Division of Parks and Recreation offer the following comments on VIN-327-12.53 (PID 75658):

By the information provided, it appears the proposed project is in close proximity to the property line of the Lake Hope State Park. If the proposed work will cross any of the Division's lands and/or waters a real estate agreement will need to be created and executed prior to the start of work on the Division's property; this includes staging equipment on the Division's lands. Please contact the Division's Real Estate Manager, Mr. Kim Caris, at 614-265-6514 well in advance of the project's start date. The Division's specific comments and requests, including land use requirements, will be incorporated in the real estate agreement.

The Division of Parks and Recreation takes the public's safety very seriously; therefore please take all necessary precautions to ensure the public is safe. The Division would prefer the construction not take place during the busy season, to disturb as few visitors as possible.

The Division expects all appropriate BMP's are implemented.

ODNR appreciates the opportunity to provide these comments. Please contact Brian Mitch at (614) 265-6378 if you have questions about these comments or need additional information.

Brian Mitch, Environmental Review Manager
Ohio Department of Natural Resources
Environmental Services Section
2045 Morse Road, Building D-3
Columbus, Ohio 43229-6693
Office: (614) 265-6378
FAX: (614) 267-4764
brian.mitch@dnr.state.oh.us



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Ecological Services
4625 Morse Road, Suite 104
Columbus, Ohio 43230
614-416-8993 / FAX 614-416-8994

December 24, 2008

Timothy M. Hill
Office of Environmental Services
Ohio Department of Transportation
P.O. Box 899
Columbus, OH 43216-0899

TAILS: 31420-2009-I-0204 (PID 75797)
31420-2009-I-0205 (PID 82356)
31420-2009-I-0206 (PID 84664)
31420-2009-I-0207 (PID 82140)
31420-2009-I-0208 (PID 23022)
31420-2009-I-0209 (PID 82998)
31420-2009-I-0210 (PID 25092)
31420-2009-I-0211 (PID 24944)

Attn: Donald Rostofer
RE: MOA and PC Project Notifications, December 19, 2008

Dear Mr. Hill,

This is in response to your December 19, 2008 MOA and PC Project Notifications letter received in our office on December 22, 2008, requesting our review and comments on 8 projects involving bridge or culvert replacements and/or roadway improvements on essentially existing alignments, and other projects that meet a Nationwide 404 Permit. These project notifications are in accordance with the Memorandum of Agreement (MOA) for Interagency Coordination for Highway Projects Which Involve Stream Crossings and/or Minor Wetland Fills and the Programmatic Consultation (PC) between USFWS, FHWA, and ODOT. We are attaching a copy of the list of projects and their locations you provided.

We note and appreciate your thorough description of project-specific conditions regarding potential Indiana bat roosting and maternity habitat and efforts to avoid impacts to such habitat. On bridge replacement projects, we recommend that, prior to any bridge removal, the underside of the bridge be carefully examined for the presence of bats, especially from April 1 to September 30. If any bats are found roosting on the underside of the bridge, please immediately contact this office to provide this information.

Regarding impacts to the **Indiana bat** (*Myotis sodalis*), the following projects fall under the PC1-a and PC1-b category of impacts within the PC and therefore will not require the use of cutting date restrictions: **HAN-CR95-0.00 (PID 75797)**, **CLA-70-13.98 (PID 84664)**, **JAC-SR93-2.94 (PID 23022)**, **NOB-566-1.19 (PID 82998)**, and **HOL-179-0.67 (PID 24944)**. The Service concurs with your determination that these projects, as proposed, may affect, but are not likely to adversely affect the Indiana Bat. We have no objection to these projects, provided that conditions in the PC are followed and that best management practices (BMPs) are implemented during and following construction activities to minimize water quality impacts and impacts to high quality fish and wildlife habitats.

Project **UNI-TR99-0.24 (PID 82356)** occurs within the Central Management Unit and falls under the PC2-c category of impacts within the PC. There is a perennial water source within 0.5 mile of potential

roost trees that will be removed for the project, the roost trees are connected to a 100-acre woodlot, and the project site is within 5 miles of a recent Indiana bat capture (August 2008) in Union County. The Service concurs with your determination that this project, as proposed, may affect, but is not likely to adversely affect the Indiana bat. However, we appreciate ODOT's commitment to follow conservation measure A-1 of the Programmatic Consultation for this project, under which trees within the project area will be cleared only between 15 September and 15 April. Please note that the Service encourages the use of the revised guidelines of tree removal between 30 September and 1 April, if possible, as Indiana bats have been observed arriving at their traditional summer areas earlier in the spring and staying longer in the fall than previously documented.

The **UNI-TR99-0.24** project proposes to replace a bridge over Mill Creek in Union County. Although your letter stated that no Federally-listed mussel species will be affected at the project site, it should be noted that the **rayed bean** (*Villosa fabalis*), a Federal Candidate species, is known to occur at other locations within Mill Creek. Therefore, Best Management Practices (BMP's) should be utilized to minimize sedimentation and erosion along the river during construction to mitigate any potential impacts to this species.

Project **CLE-28-2.05 (PID 82140)** is within the known range of the **running buffalo clover** (*Trifolium stoloniferum*), a Federally-listed Endangered Species. A survey for this species was conducted by biologists with LJB Inc. on May 19, 2008, and no individuals were found. Therefore, the Service concurs with your determination that this project, as proposed, may affect but is not likely to adversely affect the running buffalo clover.

Project **WAS-7-20.31 (PID 25092)** is within the potential range of the Federally-Endangered pink mucket (*Lampsilis abrupta*) and Federal Candidate sheepnose (*Plethobasus cyphus*) mussels and within the known range of the Federally-Endangered fanshell (*Cyprogenia stegaria*) and Federal Species of Concern snuffbox (*Epioblasma triquetra*) mussels. The Service concurs with your determination that the project may affect but is not likely to adversely affect the **sheepnose mussel**. Although it is unlikely that any of these Federally-listed species occur within the immediate project area, Best Management Practices (BMP's) should be utilized to minimize sedimentation and erosion during installation of the spiral liners to mitigate any potential impacts to these and other mussel species.

In general, the Service recommends that unavoidable impacts to streams, wetlands, and other important habitats be mitigated. On projects that include plans to replace culverts, we recommend that they be placed to allow free movement of aquatic fauna. Also, on projects that include plans to use riprap for channel protection, we recommend using native vegetation to control erosion, or, at a minimum, use native vegetation in combination with rock. To summarize, we recommend the use of natural channel design techniques where applicable. Also, on all pertinent projects, we recommend that existing riparian habitat zones be maintained to the maximum extent possible.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (48 Stat. 401, as amended; 16 U.S.C. 661 et seq.), the Endangered Species Act, of 1973, as amended, and is consistent with the intent of the National Environmental Policy Act of 1969, and the U.S. Fish and Wildlife Service's Mitigation Policy.

This concludes consultation on this action as required by section 7(a)(2) of the Endangered Species Act. Should, during the term of this action, additional information on listed or proposed species or their critical habitat become available, or if new information reveals effects of the action that were not previously considered, consultation with the Service should be reinitiated to assess whether the determinations are still valid.

If you have questions, or if we may be of further assistance in this matter, please contact Karen Hallberg at extension 23 in this office.

Sincerely,


for Mary Knapp, Ph.D.
Field Supervisor

cc: ODNR, DOW, SCEA Unit, Columbus, OH
Ohio Regulatory Transportation Office, Columbus, OH (with all attachments)

December 19, 2008

MOA Project Notifications

DISTRICT	C-R-S	PID	WATERBODY	WETLANDS
1	HAN-CR 95-0.00	75797	None	None
2	FUL-US 20/ SR 109-13.76/5.80	80672	Bad Creek, Bear Creek	None
3	ERI-Cedar Point Drive	82670	3 Un. Tribs. to Pipe Creek	<0.1 acre
3	RIC-C.R. 175-4.62	84043	Trib. of Paramour Creek	None
4	ATB-Lake Ave. Bridge	80653	None	None
6	UNI-TR 99-0.24	82356	Mill Creek	None
7	CLA-70-13.98	84664	3 Un. Tribs. to Buck Creek	None
7	MER-TR 61-25.41	83314	St. Mary's River	0.14 acre
8	CLE-28-2.05	82140	Trib to Little Miami River	None
8	HAM-SR 4-7.44	83080	None	None
9	JAC-SR 93-2.94	23022	Hunting Camp Creek	None
9	LAW-US 52-4.24	20429	Osborne Run	None
10	ATH-681-5.91	77636	Un. Trib. to Lake Snowden	None
10	NOB-83-4.19	79562	Rannells Creek, West Fork Duck Creek	None
10	NOB-147-6.10	79547	Little Buffalo Creek, Opossum Run, Un. Trib. to Opossum Run	None
10	NOB-566-1.19	82998	Opossum Run, Un. Trib. to Opossum Run	None
10	VIN-Zaleski S.F. FY 2010	84873	5 Un. Tribs. to Raccoon Creek	None
10	VIN-327-12.53	75658	Little Sandy Run	None
10	WAS-7-20.31	25092	Mile Run, Un. Trib. to West Fork Duck Creek	None
11	HOL-179-0.67	24944	3 Un. Tribs. to Crab Run	None

Projects listed in bold are projects that May Affect a federally listed species and require full USFWS review.

Ohio Department of Transportation, Office of Environmental Services
FARMLAND PROTECTION POLICY ACT
PROJECT SCREENING SHEET

I. PROJECT INFORMATION:

A. County-Route-Section: CLE-SR28-2.05

PID: 82140 **Length:** 0.350 miles

B. Brief Description:

This project involves the installation of safety improvements along State Route 28 beginning at Castleberry Court and ending at the south bound IR275 exit ramp terminal. The total project length is approximately 0.350 miles. The project is located within Miami Township, in Clermont County, Ohio. The project is situated within a sub-urban area, on level terrain, with the existing local development primarily comprising of commercial and residential development. The logical termini have been defined by the required work items necessary to install the proposed improvements. Improvements generally include the addition of a turn lane from Castleberry Court to the IR275 ramp terminals, curb and gutter, six foot sidewalks on both sides of SR28, a two-way left turn lane between Milford Hills Drive and Old Bank Road, upgrades to existing traffic signals, and improved access management. Approximately 0.694 acres of permanent right-of-way and 0.968 acres of temporary right-of-way will be required in install the given improvements. No total takes will be necessary.

C. Screening Criteria for Land to be Acquired (only one (1) needs be marked if it applies to *entire* project area; if *none* can be marked, FCIR form is required):

- Developed** with a density of at least 30 structures per 40 acres.
- Identified as “**urbanized area**” (UA) on U.S. Census Bureau Map.
- Identified as **urban area mapped with a “tint overprint”** on USGS topographical map(s).
- Identified as “**urban-built-up**” on USDA Important Farmland Map(s).
- Bridge replacement** requiring less than 1 acre of new R/W – (approx. _____ acre required).
- Widening or intersection improvement** requiring less than 3 acres of new R/W – (approx. 0.694 acres required).
- Temporary R/W** to be returned to existing or greater productive capability – (approx. 0.968 acres required).
- Channel easement** for shaping existing channel – (approx. _____ acres required).

II. CONCURRENCE:

It is hereby determined that completion of the Farmland Conversion Impact Rating form (USDA Form AD-1006) is not required because the project will not affect farmland as defined in 7 CFR Part 658, as amended, or because the project falls within the criteria in the 1984 Memorandum of Understanding between ODOT, FHWA and USDA/SCS.

 11/19/08
 District Environmental Coordinator Date



**OHIO DEPARTMENT OF TRANSPORTATION
INTER-OFFICE COMMUNICATION
Office of Environmental Services**

DATE: February 4, 2009

TO: Hans Jindal, District 8 Deputy Director
Attn: Keith Smith, District Environmental Coordinator

FROM: 
Timothy M. Hill, Administrator, Office of Environmental Services

SUBJECT: Cultural Resources Review

PROJECT: CLE-SR 28-2.05 **PID:** 82140



On January 23, 2009, The Office of Environmental Services (OES) staff completed a field inspection for the road widening project in the City of Milford, Miami Township, Clermont County, Ohio. The project will create a uniform five lane section by adding turn lanes from Castleberry Court to the I-275 ramp terminals. A two-way left turn lane will be added from Milford Hill Drive and Old Bank Road, and exclusive left turn lanes will be added at Milford Hills Drive, Old Bank Road and McClelland Road. Other improvements will include the addition of sidewalks and curb and gutter drainage on both sides of State Route 28, modification of the existing traffic signals at Castleberry Court and McClelland Road, and the construction of a short raised median east of Old Bank Road to control left-turn movements. The project may affect a concrete box culvert (no structure file number) that is located between Milford Hill Drive and the CenterBank (744 SR 28). The project will implement access management measures on the corridor that will remove or relocate several existing commercial drives that are offset from drives on the opposite side of State Route 28. The project will require approximately 0.647 acres of permanent right of way and approximately 0.642 acres of temporary right of way. The Area of Potential Effects (APE) is the footprint of the proposed project (see attached plan and profile maps).

A literature search was conducted on January 22, 2009, to determine if previously recorded sites would be affected. This information was obtained through the Ohio Historic Preservation Office's online mapping service. The literature search found no previously recorded history/architecture properties, archaeological sites and National Register resources within the project APE. However, one residential structure, a single-family residence (732 SR 28, See photos 1-8 in the coordination package), is located adjacent to the SR 28/Milford Hill Drive intersection. It is a Cape Cod style one and a half story frame structure constructed in 1950. The house is located 125 feet from the existing right of way for SR 28. Also a barn is located 280 feet from the existing right of way. No buildings or standing structures will be removed or altered by project construction. New right of way will be required (approximately 0.09 acres of permanent and 0.12 acres of temporary). The resource is not representative of an architectural style or construction method. The resource is not representative of the early settlement or farming patterns of the region. The available histories did not associate an important individual or event with the property. No further history/architecture investigations are warranted.

The culvert type (concrete box culvert) is determined not eligible for inclusion on the National Register of Historic Places pursuant to the *Programmatic Agreement Among the Federal Highway Administration, Ohio Division The Advisory Council On Historic Preservation Ohio Department of Transportation Ohio State Historic Preservation Officer Regarding Federally Funded or Approved*

Highway Bridge Projects (Agreement Number 10978) executed April 3, 2002; therefore, the bridge is not a historic property, and is exempt from further consideration.

USDA/SCS (online Web Soil Survey 2.1 <http://websoilsurvey.nrcs.usda.gov>) reports two different soils types within the project APE (see photo 1). Cincinnati silt loam (CcC2) is in six to twelve percent slope, and Rossmoyne silt loam (RpB) is two to six percent slope and is a non-hydric soil with hydric components. The field investigation concluded the area was disturbed by urban development and steep slope (see photos 2-17). Based on all the physical data, the Office of Environmental Services concludes no further work is recommended unless the scope of the project changes.

In accordance with Stipulation 4B of the *Programmatic Agreement Among The Federal Highway Administration, The Advisory Council On Historic Preservation, The Ohio Historical Society, State Historic Preservation Office, And The State Of Ohio, Department Of Transportation Regarding The Implementation Of The Federal-Aid Highway Program In Ohio (Agreement No. 12642)* executed July 17, 2006, and in compliance with 36 CFR Section 800.4(d)(1), ODOT-OES has determined that "no historic properties affected" is the appropriate finding for the proposed highway project based on the following:

- 1) No significant history/architecture resources will be effected by the undertaking;
- 2) The concrete box culvert is not eligible for the National Register of Historic Places.
- 3) No previously recorded archaeological resources are found within the APE;
- 4) The disturbed areas across the APE precludes the existence of any significant archaeological resources.

This completes the Section 106 review and no further cultural resource investigations are required. You may process the environmental document with no further comment or involvement from ODOT-OES unless the scope of the proposed undertaking were to change. The environmental document should note the date of this IOC for project Section 106 clearance. The environmental document should also note the date of the July 17, 2006 Programmatic Agreement as the basis for the Section 106 approval. A copy of this IOC should be attached to the appropriate environmental document. If you have any questions or comments regarding this determination, they may be addressed to Tara Tarlton, Staff Archaeologist at tara.tarlton@dot.state.oh.us or 614-644-7087.

TMH/ tdt

C: Mark Epstein, SHPO, w/attachments; Project File; Reading file

**United States Department of the Interior
National Park Service
Land & Water Conservation Fund
Detailed Listing of Grants Grouped by County
Ohio - 39**

Today's Date: 4/15/2008

Grant ID & Element	Type	Grant Element Title	Grant Sponsor	Amount	Status	Date Approved	Exp. Date	Cong. District
305 - XXX	A	STONELICK STATE PARK	DEPT. OF NATURAL RESOURCES	\$198,459.85	C	4/18/1974	12/31/1976	2
502 - XXX	D	SYCAMORE PARK	CLERMONT COUNTY	\$29,650.00	C	1/20/1977	12/31/1979	2
538 - XXX	D	PLUM STREET PARK	VILLAGE OF NEW RICHMOND	\$13,554.00	C	3/22/1977	12/31/1980	2
576 - XXX	C	D/MONROE TOWNSHIP PK	TOWNSHIP OF MONROE	\$18,600.00	C	7/27/1978	12/31/1980	2
665 - XXX	A	D/CROOKED RUN	DEPT. OF NATURAL RESOURCES	\$50,000.00	C	10/17/1978	12/31/1983	2
863 - XXX	C	D/LOVELAND PARK	CITY OF LOVELAND	\$86,769.92	C	2/17/1981	12/31/1986	2
954 - XXX	C	D/RIVERFEST PARK	VILLAGE OF NEW RICHMOND	\$23,915.80	C	6/24/1983	12/31/1988	2
1066 - XXX	D	UNION TOWNSHIP PARK	TOWNSHIP OF UNION	\$66,782.47	C	5/27/1986	12/31/1991	2
1136 - XXX	D	UNION TOWNSHIP PARK	TOWNSHIP OF UNION	\$86,211.00	C	6/1/1989	12/31/1993	2
1165 - XXX	D	PIERCE TOWNSHIP PARK	TOWNSHIP OF PIERCE	\$94,098.40	C	8/19/1991	12/31/1996	2
1184 - XXX	D	MIAMI TOWNSHIP PARK	TOWNSHIP OF MIAMI	\$29,354.68	C	8/26/1992	12/31/1997	2
1214 - XXX	A	KELLEY NATURE	CLERMONT COUNTY	\$92,402.00	C	3/31/1995	12/31/1999	2
CLERMONT County Total:				\$789,798.12				12



U.S. Department
of Transportation
**Federal Highway
Administration**

Ohio Division Office
200 North High Street
Columbus, Ohio 43215

September 4, 2008

Director James Beasley, P.E., P.S.,
Ohio Department of Transportation
1980 West Broad Street
Columbus, OH 43223

In Reply Refer To:
HPD-OH

Dear Director Beasley:

This letter responds to ODOT's August 19, 2008, request for a project level PM 2.5 and PM 10 conformity determination for several projects in the 2008-2011 STIP. The FHWA Ohio Division has reviewed the list and consulted with FTA Region 5 and USEPA Region 5 to determine the status of these projects.

Section 176 (c)(1)(B) states that all projects in nonattainment areas are subject to transportation conformity. The March 10, 2006 final rule requires PM 2.5 and PM 10 hot-spot analyses to be performed for projects of air quality concern. USEPA has determined that projects not identified in 40 CFR 93.123(b)(1) as projects of air quality concern have met statutory requirements and are exempt from further hot-spot analysis (40 CFR 93.116(a)).

Based upon our review of the project listing and through consultation with FTA and USEPA we find the projects on the attached list are not projects of air quality concern and that they have met the statutory requirements of the Clean Air Act and are exempt from further hot-spot analysis per 40 CFR 93.116(a). Documents prepared to satisfy NEPA requirements for the subject projects should cite this letter when discussing the status of project level conformity.

If you have any questions or comments, please contact Mr. Frank Burkett, Air Quality Specialist, at (614) 280-6838, or frank.burkett@fhwa.dot.gov.

Sincerely,

For: Dennis A. Decker
Division Administrator

**MOVING THE
AMERICAN
ECONOMY**



cc: Tim Hill, ODOT
Adam Alexander, ODOT
Frank Burkett, FHWA
David Snyder, FHWA

File: 9-M

76290	BUT	Conduct environmental analysis for proposed widening of SR 4 bypass from two lanes to 4 lanes , with turn lanes as required. and proposed Bobmeyer Road extension from Airport to SR 4B.	BUTLER CO. TID	Nonexempt	2009	24683	1217	0.047	949	26970	1330	0.047
79686	BUT	Widen Sr 122 for additional eastbound and westbound, resurface existing roadway, update signs, install storm sewers, install four new bus shelters.	Middletown, City of	Nonexempt	2009	30047	5303	0.15	4136	40783	7197	0.15
82140	CLE	Construct a 5 lane (from existing three lane) curb and gutter section with a four foot sidewalk on one side. The intersection of Castleberry Court and SR 28 will have a traffic signal installed with associated turn lanes.	MILFORD	Nonexempt	2011	28360	580	0.02	400	30330	620	0.02
79111	CLE	Widen to four through lanes with turn lanes located at signalized intersections and a landscaped median to define access locations. Service roads will be constructed to minimize curb cuts, promote safety, and define access. Project will provide for multi	CLERMONT COUNTY ENGINEER	Nonexempt	2009	16880	520	0.03	348	16880	520	0.03
5005	GRE	RECONSTRUCTION AND WIDENING OF ROADWAY TO FOUR LANES WITH LEFT TURN LANES. (MPO'S-M) DAYTON-XENIA ROAD FROM WILDONNA DR. TO E. LYNN DR.	BEAVERCREEK	Nonexempt	2009	16325	709	0.04	489	21223	922	0.04
80468	GRE	Eliminate at-grade intersections at Factory Road/Shakertown Road and Valley Road by constructing new interchanges.	DISTRICT 8 PRODUCTION	Nonexempt	2014	34408	4692	0.12	3660	40813	5564	0.12
75880	HAM	Widen US 22 to increase capacity and to improve safety (Phase 3). For PE, see PID 12377.	HAMILTON COUNTY ENGINEER	Nonexempt	2009	23300	699	0.03	545	30000	900	0.03

Noel.Alcala@dot.state.oh.us

05/21/2009 11:22 AM

To PSage@ljbinc.com

cc DNewhouse@ljbinc.com, elvin.pinckney@dot.state.oh.us,
Keith.Smith@dot.state.oh.us

Subject Re: CLE-28-2.052- PID 82140- MSAT Analysis Report

Patrick:

Because this project is not adding new through travel lanes, a new interchange, or a new road on new alignment, a Qualitative MSAT analysis is not required. Please use the MSAT Technical Guidance Appendix A prototype language in the Air Quality remarks section of the CE document. Thanks.

If you have any questions or concerns, please do not hesitate to call.

Noel Alcala, Noise and Air Quality Coordinator, P.E.
ODOT, Office of Environmental Services
Phone: 614/466/5222



**OHIO DEPARTMENT OF TRANSPORTATION
INTER-OFFICE COMMUNICATION
Office of Environmental Services**

TO: Andy Fluegemann - Planning and Prog. Admin. #8
Attn: Keith Smith P.E. - Planning Engineer

DATE: April 8, 2009

FROM: Noel Alcala, Noise and Air Quality Coordinator, OES

SUBJECT: Traffic Noise Impact Screening dated April 6, 2009

PROJECT: CLE-SR28-2.05 PID #82140

The subject Noise Screening report has been reviewed by this office and we concur with the consultant's conclusion that no additional traffic noise analysis is required for this project, since there are no future noise impacts predicted in the design year build condition.

Any questions, please call Noel Alcala of this office at 614-466-5222 or Elvin Pinckney at 614-466-5154.

NAA:EWP

cc: File

Reading file



**ODOT/OES
Community Impact Assessment Checklist**



Date: August 12, 2009

Project Information

PID: 82140 County, Route, Section: CLE-28-2.05 SJN: 487184

Brief project description:

This project will improve 0.4 mile of State Route 28 in the City of Milford, Ohio, from Castleberry Court to the I-275 interchange. The project includes the turn lanes, sidewalks, curb and gutter, and upgrades to two traffic signals.

Purpose

What is the purpose of the project? *Check all that apply*

- | | | |
|---|---|---|
| <input checked="" type="checkbox"/> Reduce congestion | <input checked="" type="checkbox"/> Improve safety | <input type="checkbox"/> System linkage |
| <input type="checkbox"/> Economic development | <input checked="" type="checkbox"/> Improve bicycle/pedestrian safety | |
| <input type="checkbox"/> Change to current design standards | <input type="checkbox"/> Other: _____ | |

Community Facilities

Are there any of the following facilities within 1/4 mile of the proposed project? *Check all that apply*

- | | | | | |
|--|-----------------------------------|---|---|--|
| <input type="checkbox"/> School | <input type="checkbox"/> Park | <input type="checkbox"/> Hospital | <input type="checkbox"/> Fire Station | <input type="checkbox"/> Community/rec center |
| <input type="checkbox"/> Church | <input type="checkbox"/> Bus Stop | <input type="checkbox"/> Police Station | <input type="checkbox"/> Public Housing | <input type="checkbox"/> Community Pool |
| <input type="checkbox"/> Grocery | <input type="checkbox"/> Library | <input type="checkbox"/> Laundromat | <input type="checkbox"/> Senior Center | <input checked="" type="checkbox"/> U.S. Post Office |
| <input type="checkbox"/> Other cultural resource (town gathering spot, historic building/monument) | | | | |

Access

Will the project alter access to any of the facilities described above?

For vehicles Yes ___ No Don't know ___
(Like driveway changes/restrictions, introduction of median, create severed road/cul-de-sac)

For pedestrian(s)/cyclists/other non-auto Yes No ___ Don't know ___
(Like jersey barrier, channeling to crossing, create severed road/cul-de-sac)

If yes for either question, please describe: New sidewalks will be installed to improve pedestrian access.

Will the project impact driveways/parking lots? Yes No ___ Don't know ___

Is there a bus/transit stop located along the project? Yes ___ No Don't know ___

How will the project change the availability or convenience of obtaining transit services?

Increase ___ Decrease ___ Stay the same Don't know ___

Community Characteristics

The project area consists primarily of:

Residential ___ Commercial Industrial ___

Please provide a brief description of the area:

(e.g. cohesive older residential, with a few commercial uses, such as a drug store)

The project corridor is zoned as general business and office districts that include strip commercial, retail and service uses fronting on State Route 28, and residential developments behind the commercial development.

Does the area surrounding the project appear to be low income? Yes ___ No Don't know ___

Does the area around the project appear to have minority populations? Yes ___ No Don't know ___

Members of the following populations observed. *Check any that apply*

African-American Asian Hispanic Native American

Other: _____ Don't know

Does the project area have considerable number of individuals from the following populations?

Elderly Children Disabled Don't know

Describe the level of pedestrian activity in the area.

High Medium Low Don't know

Is there bicycle activity in the area? Yes ___ No ___ Don't know

Community Cohesion

Will the project require relocation of the following? *Check and indicate number affected*

Residences _____ Businesses _____ Cemetary/graves _____

Community facilities (specify) _____ Don't know

Will the project create a barrier/divide an area/neighborhood? *(Create gaps by taking homes, introduce structural barriers such as bridges, jersey barriers, noise walls)*

Yes ___ No Don't know ___

Pedestrian/Bicycle Safety

- Are there currently sidewalks along the facility? Yes No N/A (new location)
- Proposed as part of the project Yes No Don't know
- Are there currently bike lanes/paths? Yes No N/A
- Proposed as part of the project Yes No Don't know
- Does the project include widening the road? Yes No N/A
- Will the project include a median to provide a crossing island refuge? Yes No Don't know
- How will the speed limit be changed? Increase Decrease Stay the same
- Proposed speed limit _____ mph

Describe other project features that may improve or decrease pedestrian/bicycle safety in the area. (e.g. crosswalks, pedestrian signals, separation of road and sidewalk, driveway restrictions)

This project will construct sidewalks on both sides of the corridor (no sidewalks are currently present) and pedestrian signals and crosswalks will be provided at the two signalized intersections.

Visual Impacts

- Are there large/mature trees located close to the existing road? Yes No
- Will the project maintain or remove existing plantings? Maintain Remove
- Is there development (houses, businesses) close to the existing road? Yes No
- Will the project include a median? Yes No
- What is the terrain like? Flat Hills
- Does the project include any roads designated as scenic byways? Yes No

Other

- Are there dense residential clusters that may need noise analysis? Yes No
- Will the project cross over or closely parallel a rail line? Yes No



**OHIO DEPARTMENT OF TRANSPORTATION
INTER-OFFICE COMMUNICATION
Office of Environmental Services**

TO: Keith Smith, District 8 Environmental Coordinator **DATE:** June 19, 2009
FROM: Timothy M. Hill, Administrator, Office of Environmental Services
SUBJECT: *404/ 401 Permit Determination*
PROJECT: CRS: CLE-SR28-2.05 PID: 82140 SJN: 487184

We have reviewed the permit determination package for the above referenced project to determine whether or not a United States Corps of Engineers (USACE) Permit will be necessary for the work completed.

The following is the appropriate permit action to be taken by the District Office and/or this office:

- No Nationwide or Individual Corps of Engineers Permit required.
- A Corps of Engineers Individual Permit will be needed.
- Project activity is covered by Part 330-Nationwide Permit Program Appendix A
NWP # 14
All conditions stated in the Nationwide Permit Program will be followed. The applicable Nationwide Permit(s) shown above and all their conditions (NWP, specific, general, regional and state) shall be included in the plans as special provisions.
- District Office shall secure necessary information to apply for permit and transmit to this office.
- A Pre-Construction Notification (PCN) is needed to process the permit determination. District please provide.
- Application for permit has been made by letter dated
- Permit No. _____ has been issued by the following:
 Coast Guard Corps of Engineers Ohio EPA
- Plans do not have enough information to make determination. District please provide.

Permit application due in OES by:

Comments: OES-WPU has reviewed the subject project. This project will impact at most 60' of ephemeral stream and no wetland. No temporary fill will be needed. Based on the information provided we have determined that the project meets the criteria for NWP#14 Linear Transportation Projects under the Nationwide Permit Program. This verification is valid until the NWPs expire on March 18th, 2012. Attached are the special provisions for the project which need to be attached to the construction plans. All conditions in the special provisions shall be adhered to during construction.

Please contact Bill Cody @ (614) 466-5198, Mike Pettegrew @ (614) 466-7102 or Matthew Perlik @ (614) 466-1937 with any questions.

TMH:WRC:MAP:mkp

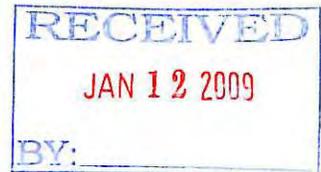
C: File – Permits File – Reading File
Date Rec: 06/17/09
NMS ✕



**OHIO DEPARTMENT OF TRANSPORTATION
INTER-OFFICE COMMUNICATION**
Office of Environmental Services

TO: Hans Jindal, District 8 Deputy Director
Attn: Katherine DeStefano
Juliet DeStefano
FROM: Timothy M. Hill, Administrator, Office of Environmental Services
SUBJECT: Environmental Site Assessment Screening

DATE: January 5, 2009



PROJECT: CLE – SR 28 – 2.05

PID: 82140

This office has reviewed the Environmental Site Assessment (ESA) Screening for the above referenced project which was completed by LJB, Inc.

Based on the information provided and a database search performed by this office, we believe that a Phase I ESA is warranted for Thorton's Gas Station (798 SR 28), Hills Real Estate Group property (749 SR 28), Former Classic Collision Repair, Inc. (731 SR 28), and Milford Oil Co. (709 SR 28).

If you have any questions or concerns, please contact Craig Kerscher, Environmental Specialist, at (614) 752-2175.

TMH:ctk

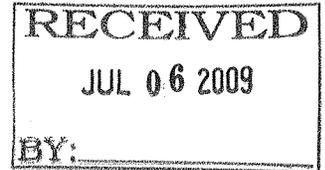
cc: Keith Smith, D-8
File w/attachment
Reading file



**OHIO DEPARTMENT OF TRANSPORTATION
INTER-OFFICE COMMUNICATION
Office of Environmental Services**

TO: Hans Jindal, District 8 Deputy Director
Attn: Katherine DeStefano -
Katherine DeStefano
FROM: Timothy M. Hill, Administrator, Office of Environmental Services
SUBJECT: Phase I Environmental Site Assessment

DATE: July 1, 2009



PROJECT: CLE – SR 28 – 2.05

PID: 82140

This office has reviewed the Phase I Environmental Site Assessment (ESA) for the above referenced project prepared by LJB, Inc.

Based on the information provided, we concur with the consultant that a Phase II ESA is warranted for the Hills Real Estate Group property (749 State Route 28). Three (3) soil borings should be advanced within the new right-of-way to a depth of 10 feet and sampled continuously. One soil sample with the highest field screen reading from each boring will be submitted for laboratory chemical analyses. Each soil sample submitted to the laboratory shall be analyzed for benzene, toluene, ethylbenzene, and total xylenes (BTEX) and methyl tertiary butyl ether (MTBE) by EPA Method 8260, polycyclic aromatic hydrocarbons (PAHs) by EPA Method 8270, and total petroleum hydrocarbon (TPH) full-carbon range organics (GRO, DRO, and ORO) by EPA Method 8015. A plan map indicating the proposed boring locations is attached to this IOC.

Based on the lab results for soil found in the BUSTR files, a plan note for petroleum contaminated soil (PCS) should be developed and placed into the plans for the Thornton's Gas Station (752 + 798 State Route 28) and Milford Oil Co. (709 State Route 28).

Based on the lab results for groundwater results found in the BUSTR file, a plan note for regulated water should be developed and placed in the plans for the Thornton's Gas Station (752 + 798 State Route 28) if dewatering is necessary in construction. In addition, low strength mortar should be used for backfill if any storm sewer work will be performed along this property.

If you have any questions or concerns, please contact Craig Kerscher, Environmental Specialist, at (614) 752-2175.

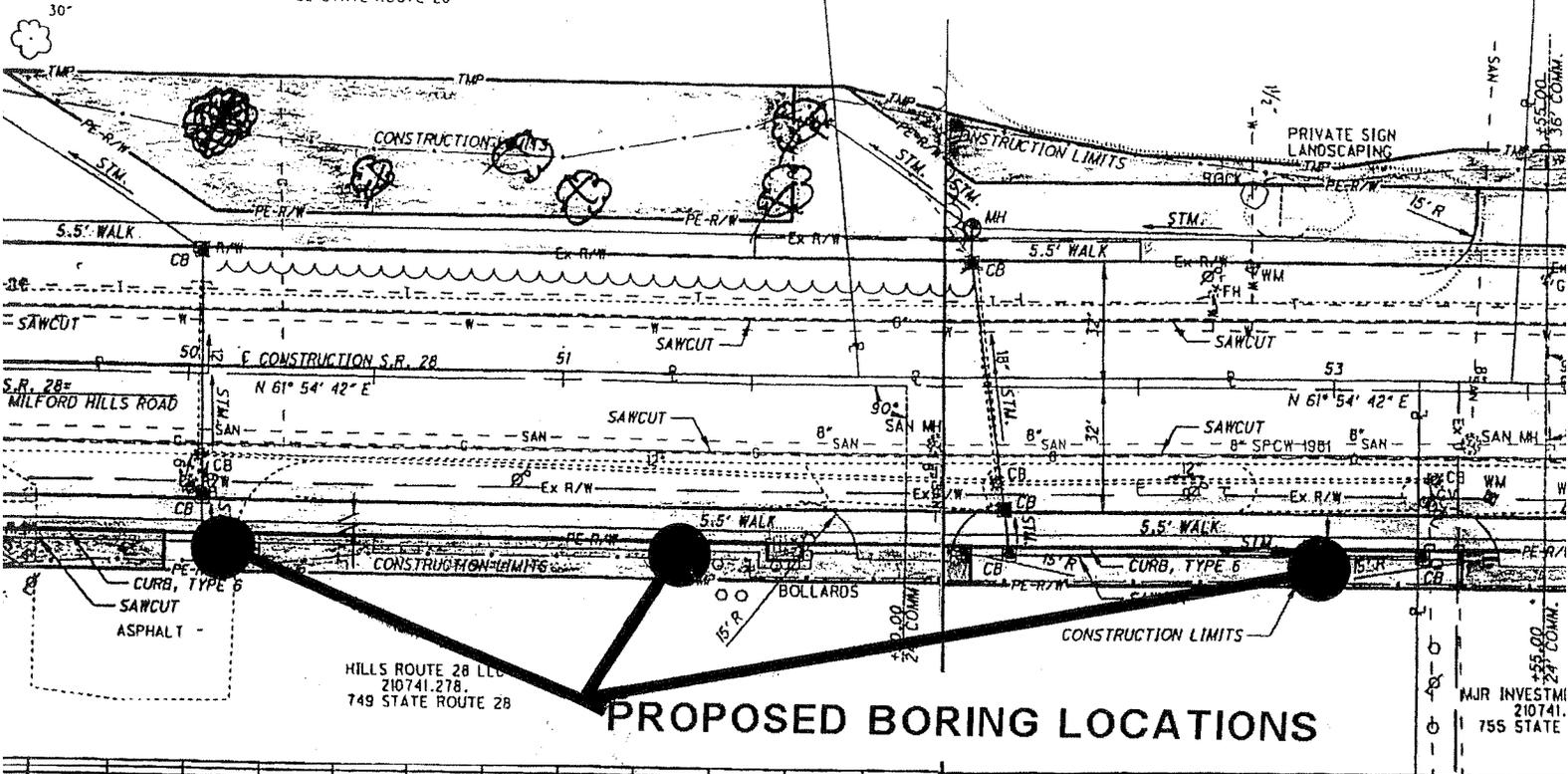
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cc: Keith Smith, D-8
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 Reading file

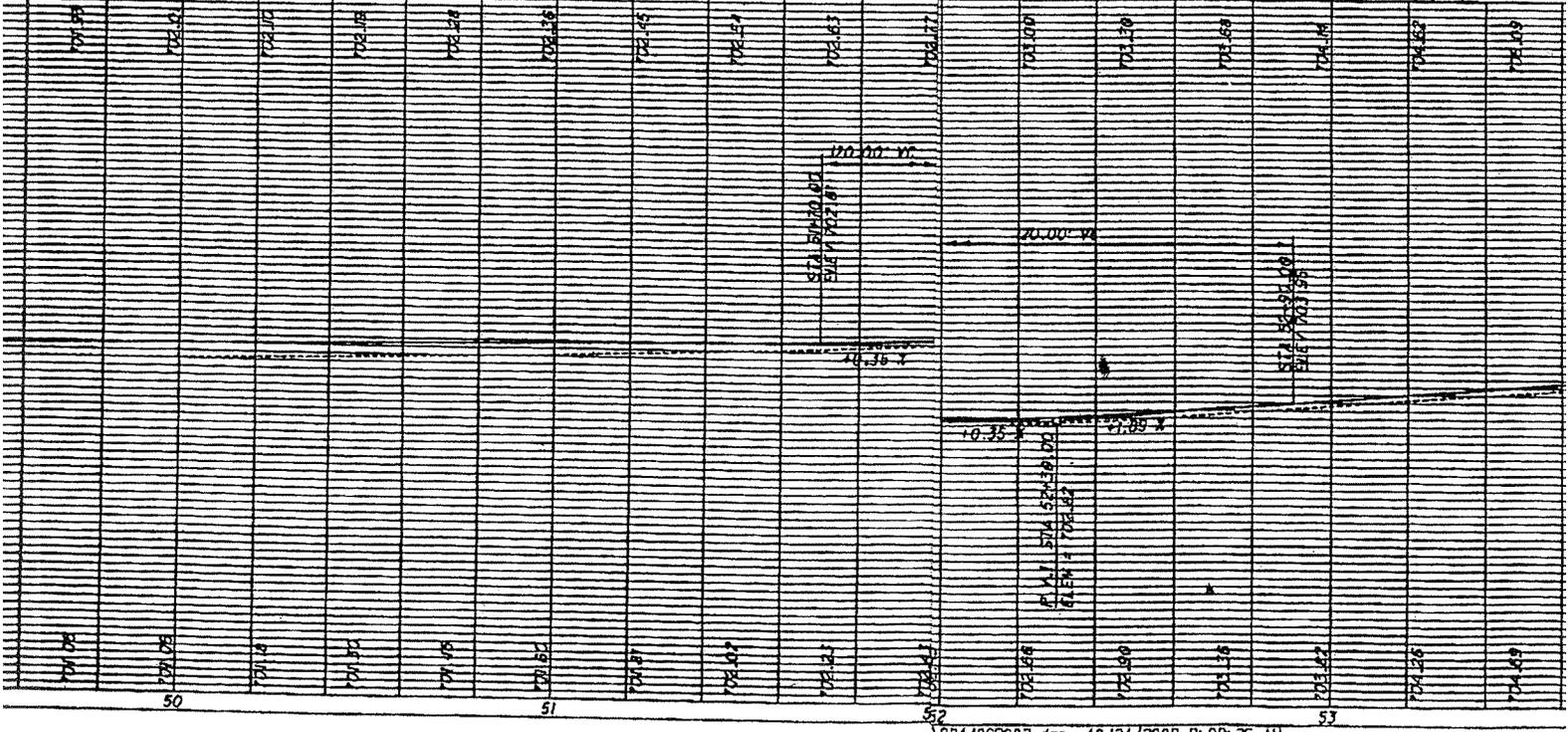
EDNA L. HENSGEN
1825130040.
732 STATE ROUTE 28

CENTERBANK
210740.292P
744 STATE ROUTE 28

ART PROPERTIES LLC
210740.293P
746 STATE ROUTE 28



PROPOSED BORING LOCATIONS



821406P003.dgn 10/31/2008 8:08:35 AM



Appendix C
Public Involvement

ADRNO	ADRSTR	OWN1	MAIL1	MAIL2	MADDR1	MADDR2	MADDR3
748	State Route 28	RJM Visions LLC	RJM Visions LLC		12165 Crestfield Ct		Cincinnati OH 45249-8252
	State Route 28	John Sora II	John Sora II		6388 Waverly Hill Ln		Loveland OH 45140-8029
	McClelland Rd	Signal Hill Partnership	Signal Hill Partnership	US Bank/Corporate Real Estate	Facilities Mgmt-Cinti East	7350 Beechmont Ave	Cincinnati OH 45230-4133
	McClelland Rd	Signal Hill Center Condo	Signal Hill Center Condo	c/o Association Advisory Group	LL9454 Kemper Grove Ln		Loveland OH 45140-8935
798	State Route 28	John Sora II	John Sora II	c/o BP America Inc	Property Tax Dept	PO Box 1548	Warrenville IL 60555-1548
750	State Route 28	Clermont Real Estate Group LLC	Clermont Real Estate Group LLC		8160 Corporate Park Dr #300		Cincinnati OH 45242-3307
746	State Route 28	Art Properties LLC	Art Properties LLC		746 State Route 28		Milford OH 45150-1831
744	State Route 28	CenterBank	CenterBank		PO Box 990		Milford OH 45150-0990
151	Old Bank Rd	BEF REIT Inc	BEF REIT Inc		3776 S High St		Columbus OH 43207-4012
732	State Route 28	Edna L Hensgen	Edna L Hensgen		732 State Route 28		Milford OH 45150-8503
101	Old Bank Rd	Round Bottom Development Company Ltd	Round Bottom Development Company Ltd		708 Walnut St Ste 700		Cincinnati OH 45202-2022
101	Castleberry Ct	Unlimited Development PLL	Unlimited Development PLL		4275 Mount Carmel Tobasco Rd		Cincinnati OH 45244-2319
	State Route 28	Round Bottom Development Comp	Round Bottom Development Company Ltd		708 Walnut St Ste 700		Cincinnati OH 45202-2022
755	State Route 28	MJR Investments 7 LLC	MJR Investments 7 LLC		2953 Wold Ave		Cincinnati OH 45206-1435
741	State Route 28	Betsy J Hall	Betsy J Hall	c/o Hali Enterprises	3671 Donata Dr		Cincinnati OH 45251-5804
749	State Route 28	Hills Route 28 LLC	Hills Route 28 LLC		4901 Hunt Rd Ste 300		Cincinnati OH 45242-6990
100	Castleberry Ct	United States Postal Service	United States Postal Service		PO Box 27497		Greensboro NC 27498-1103
	State Route 28	Milford Oil Company Inc	Milford Oil Company Inc	c/o Cecilia M Belew	546 Brandon Ave		Milford OH 45150-1208
	State Route 28	Marvin R & Karla M Theilman Trs	Marvin R & Karla M Theilman Trs		663 Lanark Ln		Loveland OH 45140-9124
703	State Route 28	Coleen M Lowe	Coleen M Lowe		109 Fieldstone Dr		Terrace Park OH 45174-1006
			Smyth Auto Parts		749 State Route 28 Ste A		Milford OH 45150-5046
			Bocca Billiards		749 State Route 28 Ste C		Milford OH 45150-5046
			Milford Animal Hospital		749 State Route 28 Ste D		Milford OH 45150-5046
			Salon Nevaeh		749 State Route 28 Ste E		Milford OH 45150-5046
			Occupant		749 State Route 28 Ste G		Milford OH 45150-5046
			Ohio Valley Center for Periodontics & Implants		748 State Route 28 Ste A		Milford OH 45150-1806
			Ohio Valley Endodontics		748 State Route 28 Ste C		Milford OH 45150-1806
			Kilbourne Medical Labs		741 Milford Hills Dr		Milford OH 45150-1446
			Occupant		741 Milford Hills Dr Ste A		Milford OH 45150-1446
			Plank, Hoseus, Ward Attorneys		741 Milford Hills Dr Ste C		Milford OH 45150-1446
			Daugherty Crown & Bridge		741 Milford Hills Dr Ste D		Milford OH 45150-1446
			Getz & Bohman Family Dentistry		741 Milford Hills Dr Ste E		Milford OH 45150-1497
			Heartland Industries		731 State Route 28		Milford OH 45150-1805
			Carworx		729 State Route 28		Milford OH 45150-1805
			Steve Wharton	Clermont County Transportation Ir	175 E Main St Ste 150		Batavia OH 45103-2973
			Occupant		755 State Route 28		Milford OH 45150-1805
			US Bank		828 McClelland Rd		Milford OH 45150-1469
			Putters Tavern		5723 Signal Hill Ct		Milford OH 45150-1483
			Occupant		752 State Route 28		Milford OH 45150-1831
			Cazadores		750 State Route 28		Milford OH 45150-1831
			CenterBank		744 State Route 28		Milford OH 45150-1831
			Occupant		798 State Route 28		Milford OH 45150-1832
			USPS		100 Castleberry Ct		Milford OH 45150-1998

Clermont County Transportation Improvement District

175 E. Main Street – Suite 150, Batavia, OH 45103

April 18, 2008

Dear Property Owner,

The Clermont County Transportation Improvement District, in cooperation with Clermont County Engineer's Office, Ohio Department of Transportation, the City of Milford, and Miami Township, is planning safety improvements to SR 28 in the City of Milford and Miami Township, Clermont County, Ohio from east of Castleberry Court approximately 0.44 miles to the SB exit ramp of I-275.

The project will include the widening of this section of SR 28 with curb and gutter cross section, to relieve congestion at the ramp locations, and the installation of a traffic signal at Castleberry Court. The project is being developed to compliment the planned ODOT safety project at the SR 28/I-275 interchange. Public Involvement is an important part of this process and you will be notified of future public meetings regarding this project.

A necessary part of this design work involves surveying, utility identification, geotechnical field investigations, soil borings, and other environmental studies for the project in the study area. Accordingly, we wish to advise you that it may be necessary for project field personnel to enter upon your property to identify environmental conditions in this area.

Field personnel representing the Clermont County Transportation Improvement District from LJB Inc., a professional engineering and environmental consulting firm, will be in the area over the next several months conducting field investigations that may require access to your property. These persons will carry identification that will be provided to you upon your request, and have received strict instructions to avoid damage to private property.

At this early stage in the design studies, there have been no definitive plans developed or final locations chosen for possible transportation improvements, nor have any detailed plans for improvements or property acquisitions been determined. Field personnel will not be able to provide any detailed information concerning the project.

The Ohio Revised Code Section 5540.03 authorizes entry onto property for these studies. If at any time you feel that our representatives have not given proper attention to your property, please notify us at once.

We sincerely appreciate your cooperation and assistance so that this important data collection work can be completed. Should you have any questions, concerns, or special needs, please feel free to contact me at (513) 289-9051. Thank you.

Sincerely,
Clermont County Transportation Improvement District



Steve Wharton
Secretary-Treasurer

Cc: Loretta Rokey, City of Milford
Board of Miami Township Trustees

OWN2	MAIL1	MAIL2	MADDR1	MADDR2	MADDR3
	USPS			100 Castleberry Ct	Milford OH 45150-1998
	Coleen M Lowe			109 Fieldstone Dr	Terrace Park OH 45174-1006
	JOHN IMBUS			116 MCCORMICK POINT	MILFORD, OH 45150
	SIGNAL HILL CENTER CONDO			11857 KEMPER SPRINGS DRIVE	CINCINNATI, OH 45240
	RJM Visions LLC			12165 Crestfield Ct	Cincinnati OH 45249-8252
	Steve Wharton	Clermont County Transportation Improvement District		175 E Main St Ste 150	Batavia OH 45103-2973
c/o Hali Enterprises	MJR Investments 7 LLC			2953 Wold Ave	Cincinnati OH 45206-1435
	Betsy J Hall	c/o Hali Enterprises		3671 Donata Dr	Cincinnati OH 45251-5804
	BEF REIT Inc			3776 S High St	Columbus OH 43207-4012
c/o Steven N. Segerman	Unlimited Development PLL			4275 Mount Carmel Tobasco Rd	Cincinnati OH 45244-2319
	Hills Route 28 LLC			4901 Hunt Rd Ste 300	Cincinnati OH 45242-6990
	Milford Oil Company Inc	c/o Cecilia M Belew		546 Brandon Ave	Milford OH 45150-1208
	Putters Tavern			5723 Signal Hill Ct	Milford OH 45150-1483
	THOMA & SUTTON EYE CARE			5724 SIGNAL HILL DRIVE	MILFORD, OH 45150
	John Sora II			6388 Waverly Hill Ln	Loveland OH 45140-8029
	Marvin R & Karla M Theilman Trs			663 Lanark Ln	Loveland OH 45140-9124
				708 Walnut St Ste 700	Cincinnati OH 45202-2022
	Round Bottom Development Company Ltd			708 Walnut St Ste 700	Cincinnati OH 45202-2022
	Carworx			729 State Route 28	Milford OH 45150-1805
	Heartland Industries			731 State Route 28	Milford OH 45150-1805
	Edna L Hensgen			732 State Route 28	Milford OH 45150-8503
c/o US Bank	ROMAR SFT PROPERTY LLC	US Bank/Corporate Real Estate	Facilities Mgmt-Cinti East	7331 AYERS ROAD	CINCINNATI, OH 45255
	Signal Hill Partnership			7350 Beechmont Ave	Cincinnati OH 45230-4133
	Kilbourne Medical Labs			741 Milford Hills Dr	Milford OH 45150-1446
	Occupant			741 Milford Hills Dr Ste A	Milford OH 45150-1446
	Plank, Hoseus, Ward Attorneys			741 Milford Hills Dr Ste C	Milford OH 45150-1446
	Daugherty Crown & Bridge			741 Milford Hills Dr Ste D	Milford OH 45150-1446
	Getz & Bohman Family Dentistry			741 Milford Hills Dr Ste E	Milford OH 45150-1497
	CenterBank			744 State Route 28	Milford OH 45150-1831
	Art Properties LLC			746 State Route 28	Milford OH 45150-1831
	Ohio Valley Center for Periodontics & Implants			748 State Route 28 Ste A	Milford OH 45150-1806
	Ohio Valley Endodontics			748 State Route 28 Ste C	Milford OH 45150-1806
	Smyth Auto Parts			749 State Route 28 Ste A	Milford OH 45150-5046
	Bocca Billiards			749 State Route 28 Ste C	Milford OH 45150-5046
	Milford Animal Hospital			749 State Route 28 Ste D	Milford OH 45150-5046
	Salon Nevaeh			749 State Route 28 Ste E	Milford OH 45150-5046
	Occupant			749 State Route 28 Ste G	Milford OH 45150-5046
	Cazadores			750 State Route 28	Milford OH 45150-1831
	Occupant			752 State Route 28	Milford OH 45150-1831
	Occupant			755 State Route 28	Milford OH 45150-1805
	Thortons Service Station			798 State Route 28	Milford OH 45150-1832
	Clermont Real Estate Group LLC			8160 Corporate Park Dr #300	Cincinnati OH 45242-3307
	US Bank			828 McClelland Rd	Milford OH 45150-1469
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	John Sora II	c/o BP America Inc	Property Tax Dept	PO Box 1548	Warrenville IL 60555-1548
	CenterBank			PO Box 990	Milford OH 45150-0990
	United States Postal Service			PO Box 27497	Greensboro NC 27498-1103

TRANSPORTATION IMPROVEMENT DISTRICT

CLERMONT COUNTY, OHIO

**SR 28 Improvements
I-275 to Castleberry Court**

Public Meeting Announcement for...

State Route 28 Improvements – I-275 to Castleberry Court

What is the purpose of this meeting?

Clermont County Transportation Improvement District (CCTID) is holding a public meeting to present proposed improvements to State Route 28 west of the I-275 interchange. Representatives from the CCTID will be present to provide information and answer questions about the project.

What is the subject of this meeting?

The State Route 28 Improvements from I-275 to Castleberry Court is a project undertaken by the CCTID in cooperation with ODOT, the city of Milford, and the Board of Township Trustees of Miami Township to address infrastructure needs along approximately 0.4 miles of State Route 28 in Miami Township.

When and where?

When: Thursday December 11, 2008
5:30 to 7:00 PM

Where: Milford City Hall
745 Center Street

The CCTID is performing this project to address traffic operations and infrastructure needs on this corridor. To that end, the project development is focused on the following GOALS:

- increasing safety and reducing traffic congestion along the State Route 28 corridor,
- complementing adjacent studies and projects under development by the CCTID,
- and minimizing new right-of-way needs while delivering improvements that serve your growing community.



Clermont County Transportation Improvement District
175 E Main St Ste 150
Batavia OH 45103-2973

Name
Address
Address
Address

Contacts

Steve Wharton

Clermont County Transportation Improvement District
175 E Main St Ste 150
Batavia OH 45103-2973
ed3c@fuse.net
513.289.9051

tid.clermontcountyohio.gov

Loretta Rokey

City of Milford
745 Center Street, Suite 200
Milford, Ohio 45103-1959
lrokey@milfordohio.org
513.831.4192

www.milfordohio.org

Dave Duckworth

Miami Township
6101 Meijer Dr
Milford OH 45150-2189
dave.duckworth@miamitwpoh.gov
513.248.3725

www.miamitwpoh.gov

The Council of the City of Milford met in Regular Session in Council Chambers at 745 Center Street at 7:00 p.m. on November 18, 2008. Vice-Mayor Hinners called the meeting to order with the Pledge of Allegiance and a Moment of Silence.

Roll Call: Present: Vice-Mayor Hinners, Mr. Vilardo Jr., Mr. Hawkins, Ms. McBeath, and Ms. Brewer. Mayor Hunter and Mr. Pschesang were absent. Ms. McBeath made the motion to excuse Mayor Hunter and Mr. Pschesang, seconded by Ms. Brewer. All voted yes.

Remarks from the Mayor: Vice-Mayor Hinners welcomed Boy Scout Troop 415 who were observing the Council meeting and also the employees who will receive proclamations for their 25 plus years of service.

Proclamations: Vice-Mayor Hinners presented proclamations to Myrt Allen, Cemetery Clerk, Carol Royer, Wastewater Supervisor and Russ Tieman, Cemetery Overseer. These employees have all had 25 or more years of service with the City of Milford. City Manager Rokey and Law Director Mike Minniear acknowledged their service.

Correspondence: The Clerk read correspondence from the Greater Milford Historical Society inviting City Manager Rokey and Council members to the Society's Holiday Party at the Terrace Park County Club on December 4th at 6:30pm.

Financial Statements: None

Public Comments: Chris Hamm from the Downtown Milford Association reminded residents of the Hometown Holidays that will be held on November 28-30. Many activities have been planned for those attending.

STANDING COMMITTEE REPORTS

Public Services: No Report

Administrative Services: Mr. Hawkins reported that the Committee met on November 12, 2008. Present: Charlene Hinners and Bryan Hawkins. Staff present: City Manager Rokey and Director of Finance Tim Petric.

The Manager provided a draft retirement proposal for Mike Haight, Service Superintendent. There was discussion of each item and a revision for clarification was suggested for Item #4. There were several scenarios discussed and the final approval is subject to review by the Law Director.

There was also a brief discussion of the city's vacation accumulation policy.

The Committee agreed to make a motion to draft an Ordinance authorizing a retirement agreement with Mike Haight, Service Superintendent. Mr. Hawkins made the motion, seconded by Ms. Hinners. Voting yes: Mr. Hawkins and Ms. Hinners. Abstaining: Ms. McBeath. Voting no: Mr. Vilardo, Jr. and Ms. Brewer. Motion failed.

The Manager provided copies of City Council's original budget and expenditures year-to-date 9-30-08. Line item details were discussed and will be provided, particularly for the miscellaneous category.

Requests have been received to renew our Clermont Chamber/Cincinnati Chamber memberships and Milford Miami Township Chamber sponsorship. City Council was encouraged to review their budget plans for next year.

The Finance Director reported that he has been meeting with Department Heads to review their operating budgets and capital requests. These will also be reviewed with the Manager. The 3rd quarter report is also being prepared and will include October.

The temporary budget must also be prepared by the end of November for adoption in December to allow operations after January 1st until the permanent budget is adopted.

The Manager discussed plans to recognize city staff with 25 or more years of service at the City Christmas party on December 19th at the Fireman's Hall.

Safety Services: No Report

Community Development: No Report.

Manager's Report: City Manager Rokey reported that the Street/Service Department has started the Vac-All on it's round through the City. Thirty-five loads have been picked up so far. Brush continues to be collected on a call-in basis. The holiday lighting has all been completed. The streets will be swept prior to Hometown Holidays. The merchants are experimenting with speakers on Main St. playing holiday music. The Fire Department will be coordinating installation using the street trees.

The curb and ADA retro-fitting at Main St. has been completed. The Mill Street modifications will probably be operational by the end of the week. Once open, traffic will be permitted to travel westbound from Cleveland to Mill.

All foundations have been completed in the cemetery for the year. The remainder of the Department will attend pesticide training to be certified. The entire Department will then be certified in Pesticide Training. The salt trucks are ready to go.

The Wastewater Department is in normal operation.

The Water Department installed a 6" 'gate valve' for a new hydrant at Main and High Streets. They also installed 30 radio read meters throughout the City. The Water Department Contingency Plan and 5 Year Capital Improvement Plan is in the process of being updated.

The Utility Department has issued shut-off notices reminding customers that due to the Thanksgiving Holiday we will begin shutting water off due to non-payment on November 24th at 8:30am. Administrative offices will be closed November 27th and 28th.

The Finance Department will be attending a year-end closing procedure meeting with SSI our software company on December 12th.

Rumpke will pick up all trash and recycling on your regularly scheduled day during the Thanksgiving Holiday. The City Fall Litter Collection on October 25th had 179 volunteers with 151 bags of garbage and 101 bags of recyclable material.

Committee Meetings

Community Dev.	Monday, November 24, 2008	5:30pm
Public Services	Tuesday, November 25, 2008	6:30p.m.
Admin. Services	week of Dec. 8 th and 15 th	

Reminders

Hometown Holidays	Nov. 28 th – Nov. 30 th	
Victorian Christmas	Thursday, Nov. 28 through Mid-January	Promont
Taste of Christmas	December 3, 2008 (Fireman's Hall)	6:30-8:30p.m.
Holiday Homecoming	Thursday, December 4, 2008 (Eastgate Holiday Inn)	5-8p.m.
Parks & Recreation	Monday, December 8, 2008	5:30p.m.
Planning Commission	Wednesday, December 10, 2008	6:00p.m.
Public Meeting – TID	Thursday, December 11, 2008 (Council Chambers)	5:30-7:00p.m.
Citizen's Housing	Monday, December 15, 2008	7:00p.m.
BZA	Thursday, 18, 2008	7:00p.m.
City Holiday Luncheon	Friday, December 19, 2008 (Fireman's Hall)	11:00-2:00
Santa Sunday	Sunday, December 21, 2008	12:30-4:00p.m.

Police Report: Chief Machan reported that a promotional exam for Sergeants would be held on November 22nd. The Department will again participate in the Drink and Drive program with Hamilton Co. during the holidays.

Fire/EMS Report: Chief Cooper distributed the monthly report to Council. The Department is busy helping with Hometown Holidays and putting up the Christmas decorations. The Chief announced that Santa Sunday would be on December 21st from 12:30 until 4:00.

Flu shots are still available to City Employees on December 15th from 5:30-7:00.

Council Comments: None

New Business: None

Ordinances and Resolutions:

Old Business: None

Ordinances and Resolution

Adjourn:

There being no further business to come before the regular Council meeting the meeting was adjourned at 7:30p.m. with a motion from Ms. McBeath, seconded by Mr. Hawkins. All voted yes.

Joanne Trilety, Clerk

David M. Hunter, Mayor



BOARD OF COUNTY COMMISSIONERS
CLERMONT COUNTY, OHIO

ROBERT L. PROUD R. SCOTT CROSWELL III EDWIN H. HUMPHREY

Immediate Release

November 26, 2008

Open House Set for Proposed State Route 28 Improvements

Batavia, Ohio. Citizens of Milford and Miami Township are invited to attend an open house meeting on Thursday, December 11, 2008, between 5:30 p.m. and 7 p.m., concerning proposed improvements to State Route 28 from Castleberry Court to I-275. The meeting will be held at Milford City Hall, located at 745 Center Street in Milford, and is sponsored by the Clermont County Transportation Improvement District (CCTID) - in cooperation with the Clermont County Engineer's Office, Miami Township, Milford, and LJB, Inc.

Proposed improvements include the widening of existing SR 28 from three lanes to five lanes, curb and gutter drainage, and the addition of sidewalks.

This open house meeting is an opportunity for citizens to provide comments and suggestions about the improvements; an exhibit of the project will be on display, and project representatives will be on hand to answer questions. Comment sheets will be available and can be sent to Craig Stephenson at the Clermont County Engineer's Office; 2381 Clermont Center Drive, Batavia, Ohio 45103; comments can be submitted until December 24, 2008.

Because this meeting is in an open house format, citizens are invited to stop by anytime between 5:30 p.m. and 7:30 p.m.

For more information about the SR 28 Corridor Improvements Study, contact Steve Wharton with the Clermont County Transportation District at (513) 289-9051 or e-mail ed3c@fuse.net.

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For additional information about this or other county news, contact Clermont County Communications Director Kathy Lehr at (513) 732-7597 or by e-mail, klehr@co.clermont.oh.us.

Open House Set for Proposed State Route 28 Improvements

Immediate Release

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#

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Intercommunity Cable Regulatory Commissions (I.C.R.C.)

I.C.R.C. Schedule for the airing of the CCTID meeting from December 11.

+	Wed	1/7/09	5:30 PM	Channel24
+	Thu	1/8/09	10:00 AM	Channel 4
+	Thu	1/8/09	8:00 PM	Channel 4
+	Fri	1/9/09	4:00 PM	Channel 4
+	Sat	1/10/09	6:30 PM	Channel 4
+	Sun	1/11/09	1:30 PM	Channel24
+	Mon	1/12/09	6:30 PM	Channel 4
+	Tue	1/13/09	6:00 PM	Channel 4
+	Wed	1/14/09	5:30 PM	Channel24
+	Wed	1/14/09	9:30 PM	Channel 4

Channel 24 is seen throughout the greater Cincinnati area. It is shared with other local Cable Access organizations and is programmed by the ICRC on Sundays and Wednesdays.

Channel 4 is programmed full time with locally produced ICRC productions. This programming can be seen only in ICRC member communities, of which the city of Milford is a part.

TRANSPORTATION IMPROVEMENT DISTRICT

CLERMONT COUNTY, OHIO

**SR 28 Improvements
I-275 to Castleberry Ct**

Introducing the...

State Route 28 Improvements – I-275 to Castleberry Court

What is it?

The State Route 28 Improvements from I-275 to Castleberry Court is a project undertaken by the Clermont County Transportation Improvement District (CCTID) in cooperation with ODOT and the Boards of Township Trustees of Miami Township to address infrastructure needs along approximately 0.5 miles of State Route 28 in Miami Township.

Why am I receiving this flyer?

This flyer is being provided to residents and businesses along State Route 28 within the identified project area (depicted below) to inform you about the project.

What happens next?

At this point, LJB Inc., a professional engineering and environmental consulting firm, will be conducting field investigations in the area over the next several months to assist in developing a plan for improvements in the project area. As the project process continues, you can expect to see more detailed information about the project and how you can get involved.

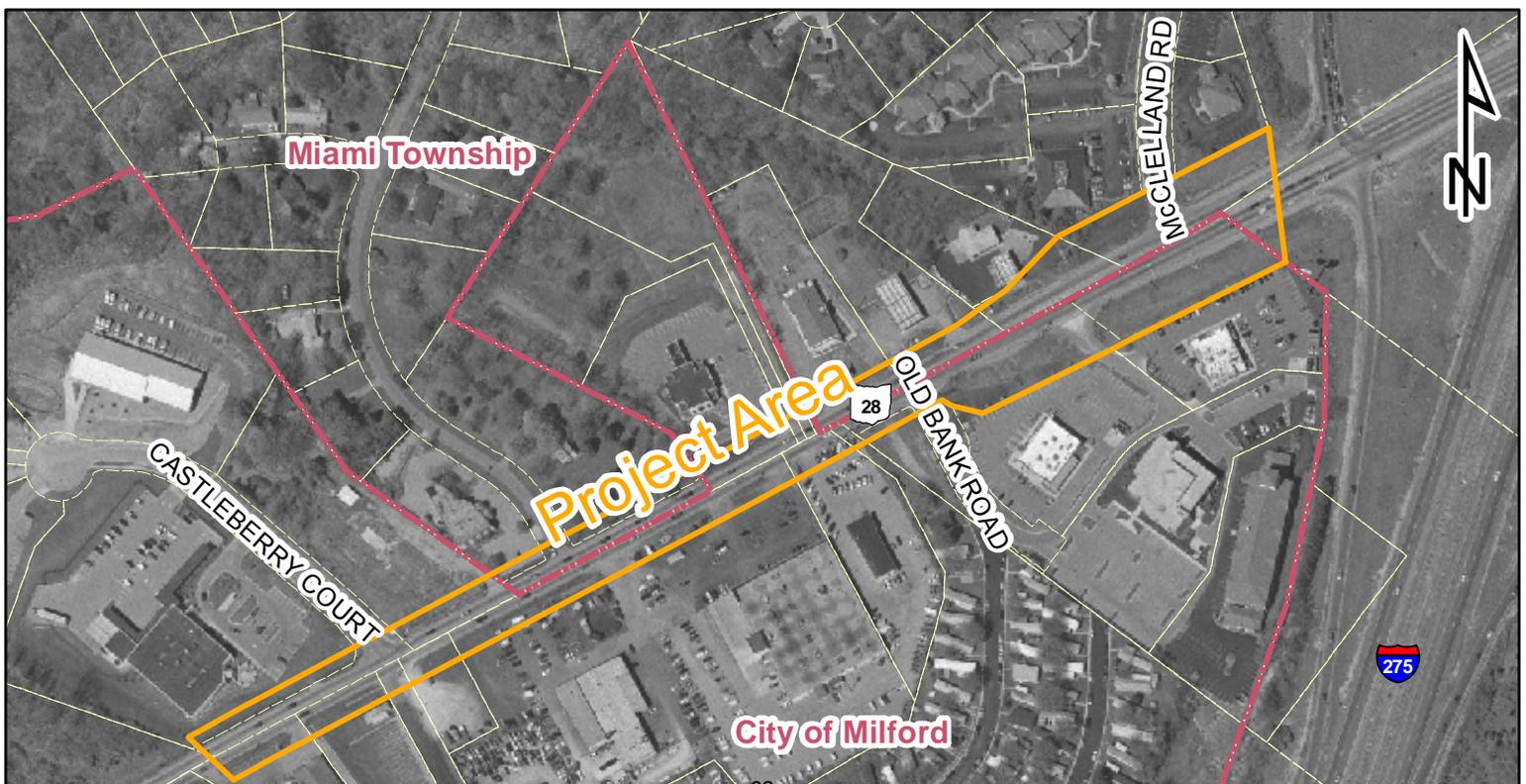
The CCTID is performing this project with the following initial GOALS in mind.

To address traffic operations and infrastructure needs for the corridor with a focus on:

- Increasing safety and reducing traffic congestion along the SR 28 corridor.
- Encourage and plan for economic development opportunities in the area.

To complement adjacent studies and projects being developed by the CCTID.

To minimize new right-of-way needs while delivering improvements needed to serve your growing community.



Clermont County Transportation Improvement District
175 E Main St Ste 150
Batavia OH 45103-2973

Name
Address
Address
Address

Contacts

Steve Wharton

Clermont County Transportation Improvement District
175 E Main St Ste 150
Batavia OH 45103-2973
ed3c@fuse.net
513.289.9051

tid.clermontcountyohio.gov

Pat Manger

Clermont County Engineer
2381 Clermont Center Dr
Batavia OH 45103-1959
pmanger@co.clermont.oh.us
513.732.8857

www.cceo-oh.org

Dave Duckworth

Miami Township
6101 Meijer Dr
Milford OH 45150-2189
dave.duckworth@miamitwpoh.gov
513.248.3725

www.miamitwpoh.gov

CITY OF MILFORD

Type of Meeting SR 28 Project Date 12-11-08

NAME	ADDRESS/AFFILIATE	DO YOU WISH TO SPEAK
1. <u>Loretta Rekey</u>	<u>City of Milford.</u>	<u>N/A</u>
2. <u>Steve Rook's</u>	<u>Burger King</u>	
3. <u>Suzah Churchill</u>		
4. <u>Don E. Rook</u>	<u>Q.S.C., I DBA Burger King</u>	
5. <u>Matt Figgins</u>	<u>CCMR GROUP 151 CASTLEBERRY CT</u>	
6. <u>John DeJager</u>	<u>714 Lanterpost</u>	
7. <u>Gene Bjinning</u>		
8. <u>Katherine J. DeJager</u>	<u>O.DOT - DOS - Planning.</u>	
9. <u>Marge Judge</u>	<u>765 Twin Fox Dr (Mann's Truck)</u>	
10. <u>Bob Judge</u>	<u>765 Twin Fox Dr (Mann's Truck)</u>	
11. <u>Veronica B. B...</u>	<u>155 SR 28</u>	
12. <u>Dale FROE</u>	<u>Roubothan Develop owns Property</u>	
13.	<u>Old Bank Road Rooster Rest</u>	
14. <u>Scot Lahrmer</u>	<u>Board of County Commissioners</u>	
15. <u>Chris Impus</u>	<u>Milford Storage</u>	
16. <u>John Impus</u>	<u>" "</u>	
17. <u>Ray & Linda Sebastian</u>		
18. <u>Bandy Cooper</u>	<u>The Myers & Cooper Co.</u>	
19. <u>ANDY JOHNS</u>		
20. <u>Ed Humphrey</u>	<u>, 101 E MAIN, BATAVIA, OH 45103</u>	

CITY OF MILFORD

Type of Meeting _____ Date _____

NAME	ADDRESS/AFFILIATE	DO YOU WISH TO SPEAK
1. Tom + Marilyn Bowers	797 Twin Fox Dr	No
2. Silvio LoUlette	Faith Church 5910 Pine Rd, Milford	NO
3. Tim Whitworth	Miami Twp	No
4. Ed & Nancy Lowe	MIAMI TWP	No
5. KARL SCHULTZ	MIAMI TWP TRUSTEE	NO
6. Jan Ackerman	Lehr's	No
7. Chris + John Christensen	774 Twin Fox	No
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CITY OF MILFORD

Type of Meeting Rt. 28 Project Date 12/11/08

NAME	ADDRESS/AFFILIATE	DO YOU WISH TO SPEAK
1. <u>SUZY T. HALL</u>	<u>773 Twin Fox Dr.</u>	<u>very informative THANK YOU!</u>
2. <u>MARY SCHUTTE</u>	<u>775 McLELLAND Rd</u>	
3.		
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SR 28 Improvements I-275 to Castleberry Court

December 11, 2009

Public Meeting

Name: NANCY LOWE
Address: 778 TWIN FOX DR
Phone: 248-9565
e-mail: _____

Comments:

- 1) POSSIBLE TO INCLUDE DEDICATED RIGHT TURN LANE OFF McCLELLAND ONTO RT 28 ?
- 2) FIX HUGE POTHOLE @ RIGHT TURN LANE FROM 28 INTO McCLELLAND ?

Send To:

Craig M. Stephenson, P.E.
2381 Clermont Center Drive
Clermont County Engineer
Batavia, Ohio 45103
513-732-8883
cstephenson@co.clermont.oh.us

From: Stephenson, Craig
Sent: Wednesday, April 08, 2009 9:57 AM
To: 'mary schutte'
Cc: Manger, Pat
Subject: RE: McClelland Road / SR28 Interchange

Mary,

Sorry I took so long to respond, but I wanted to evaluate your request thoroughly, which required some additional traffic analysis. The following is a summary of the evaluation.

The request for a turn lane was evaluated. At this signal with existing roadway conditions, the Level of Service for this movement operates at "D" in the AM peak hour and "E" in the PM peak hour. Once this improvement is implemented these Levels of Service will be raised to "C" for both AM & PM peak hours in the Design Year of 2031. The request is understandable as the existing Levels of Service for this intersection are unsatisfactory. However with the proposed improvements at this intersection, this movement will be raised to the acceptable "C" level with 2031 Design Year Traffic. In the intervening years until 2031, operation will be slightly better as traffic volumes will be less.

Traffic lanes are **required** to be **justified** by capacity analysis in order to be funded and constructed when using federal funds administered through the Ohio Department of Transportation. Our evaluation of the McClellan intersection with the planned improvements indicate that the traffic capacity analysis for the proposed improvements do not support the need nor justify a southbound right turn lane on McClelland Road.

While not a part of our project, the improvements ODOT is planning for the I-275 interchange will also enhance traffic operations on this section of SR 28 through the elimination of the left hand turn movements at the ramp intersection to southbound I-275.

While I realize the above response does not satisfy your request, I hope that you can appreciate the procedures that we, as professional engineers, must follow when designing and constructing roadway improvements.

Please contact me at 513-732-8883 with any further questions you may have.

Sincerely,

Craig

From: mary schutte [mailto:maryschutte@hotmail.com]
Sent: Monday, February 16, 2009 2:23 PM
To: Stephenson, Craig
Subject: McClelland Road / SR28 Interchange

Craig,

I met you at the public meeting that was held in at the Administrative Building in Milford back in December. The purpose of the meeting was to discuss the improvements to SR 28, from I-275 to Castleberry Court. The changes to the McClelland Road/SR 28 interchange were discussed.

The purpose of this email is to express my concerns about ensuring there is a turning lane on McClelland Road that would allow for drivers to make a right hand turn onto SR 28 while cars may be stopped at a red light, waiting to turn left. When I expressed this during the meeting I was told that this right hand turn lane was discouraged because there is not enough roadway from the exit ramp off I-275 to McClelland Road, implying that drivers turning right out of McClelland Road do not have enough distance on their left to judge cars turning right off the exit ramp and heading south on SR28. (I hope that made sense.....)

As a motorist pulling out of McClelland Road many times a day, I have never had a problem judging the traffic from the left as I turn right on red at the light.

Another argument that was presented at the meeting, to imply there is not a need for the turning lane on McClelland Road, was the fact that the lights would be re-synchronized and there would not be traffic stopped on SR28 at the I-275 light and therefore the traffic turning left out of McClelland Road would flow better and not get stopped due to the red light on SR28 at I-275. I understand that re-synchronization would improve the flow of traffic turning left out of McClelland, but the traffic turning right out of McClelland Road still gets stopped behind cars turning left. These cars turning right would benefit greatly with a right-turn lane.

Even with the configuration today, drivers do everything they can to turn right on red, often pulling onto the shoulder to go around cars waiting at the light to turn left.

I am requesting that considerable thought be given to having a right turn lane out of McClelland Road onto SR 28. A dedicated turning lane would be much safer than having drivers trying to squeeze their way into turning right when traffic is stopped at the red light, like they do now.

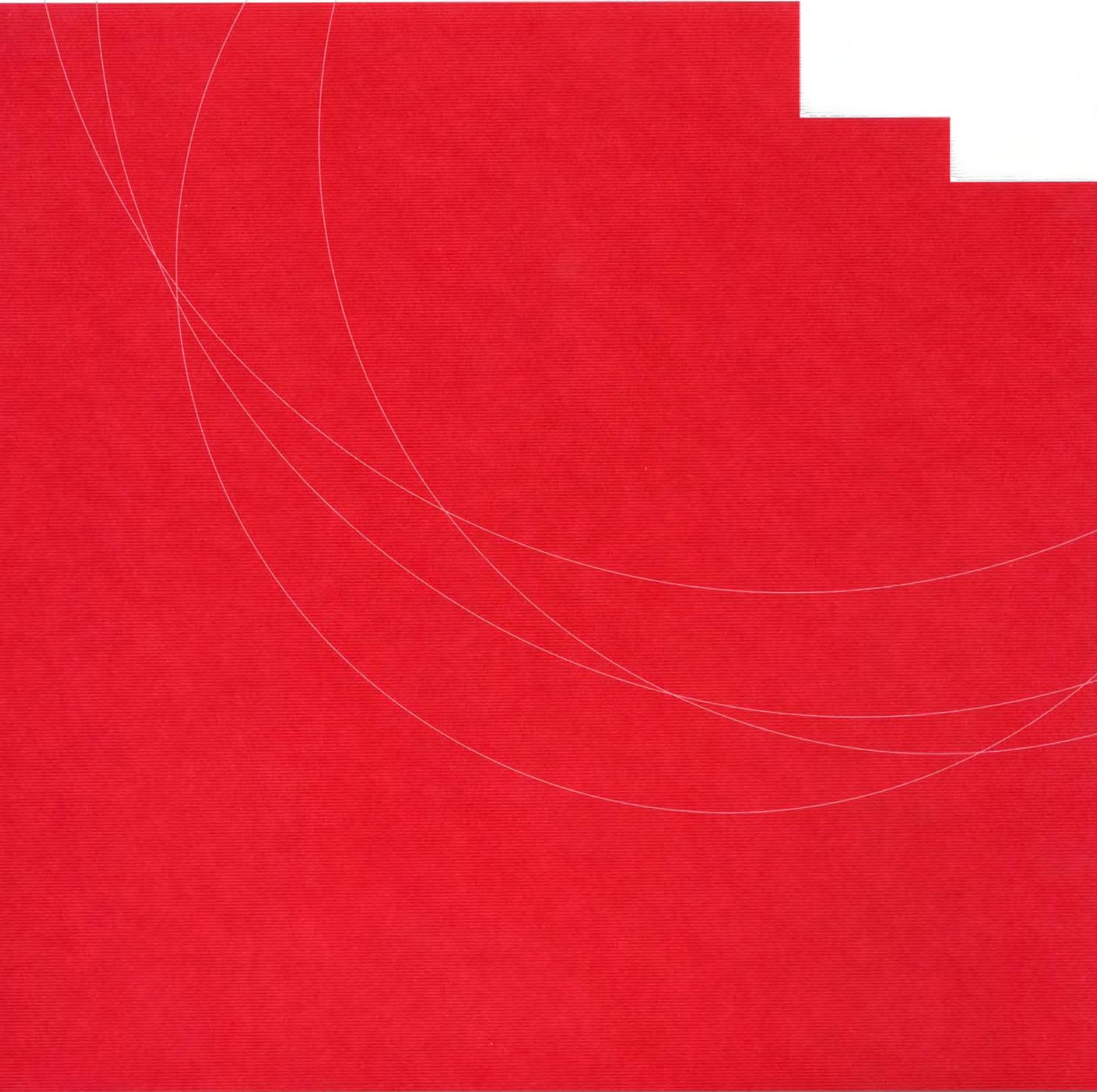
Craig, I hope my cumbersome wording here make sense. The bottom line is that I truly believe that a right turn lane from McClelland Road to SR28 would be a great benefit to the flow of traffic and would be much safer.

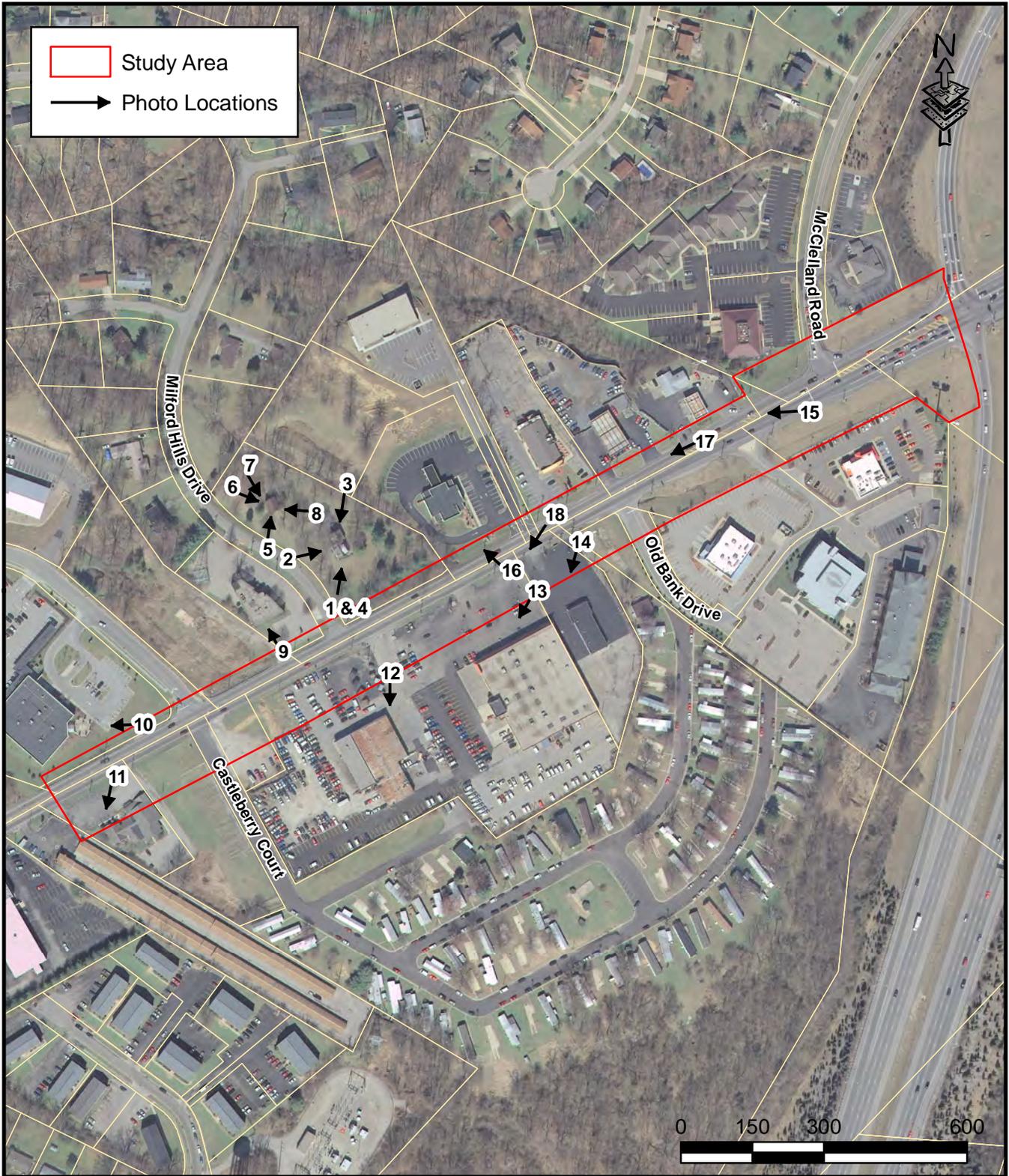
Please don't hesitate to call if I can be of any assistance in the future.

Mary Schutte
775 McClelland Road

513-248-1055

Appendix D
Photolog






LJB Inc. • 3100 Research Blvd. • P.O. Box 20246
 Dayton, OH 45420-0246
 (937) 259-5000 tel • (937) 259-5100 fax • ljbinc.com

Photo Locations and Directions

CLE-28-2.05: Castleberry Court to I-275
Milford, Miami Township, Clermont County, Ohio



Photo 1: 732 State Route 28



Photo 2: 732 State Route 28



Photo 3: 732 State Route 28 (note copper roof and stone around garage)



Photo 4: 732 State Route 28 (note stone fireplace)



Photo 5: Barn on 732 State Route 28 property



Photo 6: Barn on 732 State Route 28 property



Photo 7: Close-up of supports for barn on 732 State Route 28 property



Photo 8: Barn on 732 State Route 28 property



Photo 9:



Photo 10:



Photo 11:



Photo 12:



Photo 13:



Photo 14:



Photo 15:



Photo 16:



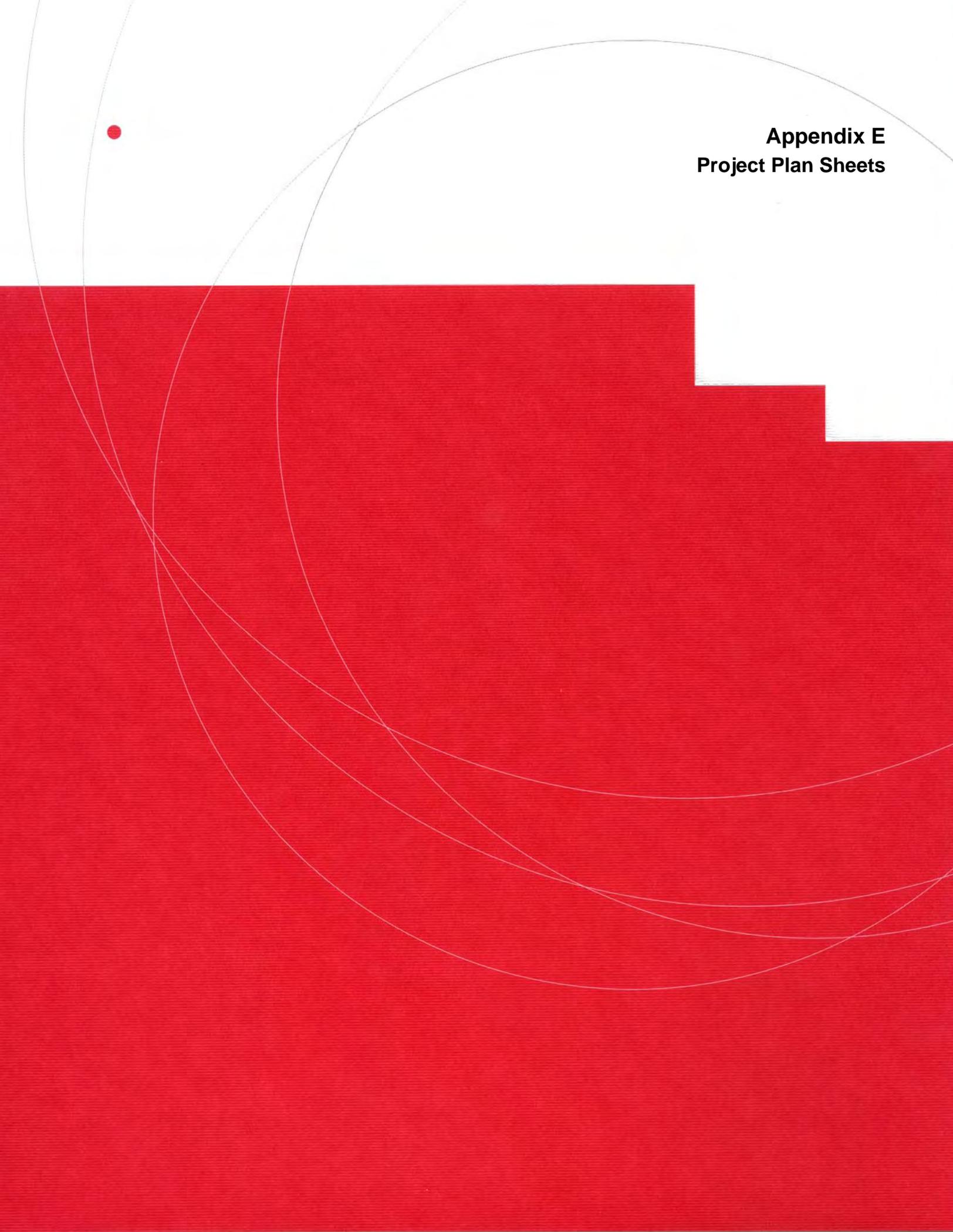
Photo 17:



Photo 18:



Appendix E
Project Plan Sheets



STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

CLE-28-2.05

CITY OF MILFORD
MIAMI TOWNSHIP
CLERMONT COUNTY

PROJECT DESCRIPTION

THIS PROJECT IMPROVES APPROXIMATELY 0.4 MILES OF S.R. 28 FROM CASTLEBERRY COURT TO THE RAMPS AT I-275 TO A 5 LANE URBAN SECTION. TRAFFIC SIGNAL MODIFICATIONS AT CASTLEBERRY COURT AND MCCLELLAND ROAD ARE INCLUDED.

PROJECT EARTH DISTURBED AREA: 5.754 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 1.000 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA: 4.754 ACRES

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2008 SPECIFICATIONS

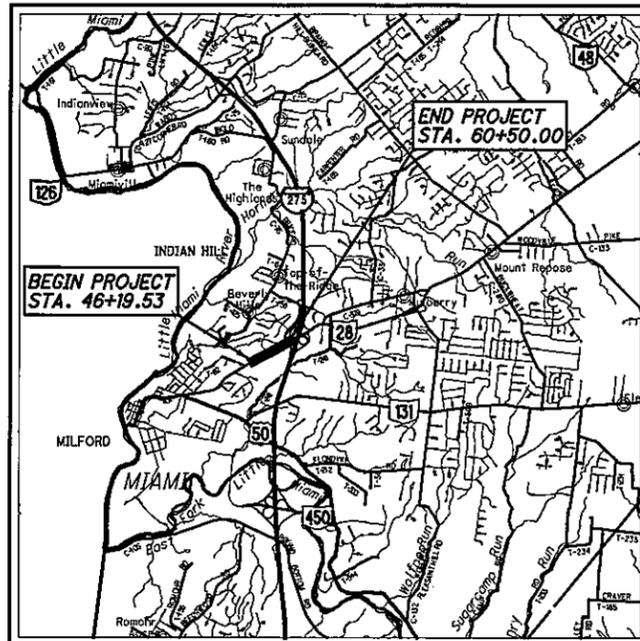
THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

**STAGE 2 SUBMITTAL
JULY 15, 2009**

APPROVED _____
DATE _____ DISTRICT DEPUTY DIRECTOR

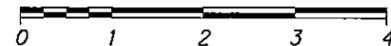
APPROVED _____
DATE _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION



LOCATION MAP

LATITUDE: 39°11'00" LONGITUDE: 84°16'10"

SCALE IN MILES



PORTION TO BE IMPROVED _____
INTERSTATE & DIVIDED HIGHWAY _____
UNDIVIDED STATE & FEDERAL ROUTES _____
OTHER ROADS _____

DESIGN DESIGNATION
SEE SCHEMATIC PLAN

DESIGN EXCEPTIONS
NONE REQUIRED

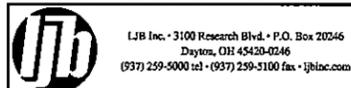
INDEX OF SHEETS:

TITLE SHEET	1
SCHEMATIC PLAN	2
TYPICAL SECTIONS	3-4
GENERAL NOTES	5
MAINTENANCE OF TRAFFIC	6-29
S.R. 28 PLAN AND PROFILE	30-34
MILFORD HILLS DRIVE PLAN AND PROFILE	35
SIDE ROADS PROFILES	36
S.R. 28 CROSS SECTIONS	37-49
MILFORD HILLS DRIVE CROSS SECTIONS	50
PAVEMENT ELEVATION TABLE	51-52
INTERSECTION DETAILS	53-56
DRIVE DETAILS	57-61
WATER WORK	62-69
TRAFFIC CONTROL	70-81
RIGHT OF WAY PLANS	82-93
SOIL PROFILE	

UNDERGROUND UTILITIES

CONTACT BOTH SERVICES
CALL TWO WORKING DAYS
BEFORE YOU DIG
CALL
1-800-362-2764
(TOLL FREE)
OHIO UTILITIES PROTECTION SERVICE
NON-MEMBERS
MUST BE CALLED DIRECTLY
OIL & GAS PRODUCERS PROTECTIVE
SERVICE CALL: 1-800-925-0988

PLAN PREPARED BY:



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ENGINEERS SEAL:

SIGNED: _____
DATE: _____

STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS					
BP-3.1	10/19/07	DM-1.1	4/21/06	WQ-1.3	10/17/08	TC-51.12	4/20/01	TC-85.21	3/06/09	878	4/18/08
BP-4.1	7/16/04	DM-1.2	10/21/05			TC-52.10	1/19/07				
BP-5.1	7/28/00	DM-4.3	4/17/09	HL-20.11	1/19/07	TC-52.20	1/19/07				
BP-6.1	7/28/00			HL-30.11	4/17/09	TC-65.10	1/21/05				
BP-7.1	1/19/07	MH-1.2	1/20/06	HL-30.21	1/19/07	TC-65.11	1/21/05				
				HL-30.22	4/17/09	TC-71.10	1/16/09				
CB-1.1	7/15/05	RM-1.1	7/18/08			TC-73.10	1/19/01				
CB-1.2	7/15/05			TC-21.20	1/16/09	TC-81.10	4/17/09				
CB-2.1	7/15/05			TC-41.10	10/19/07	TC-81.21	4/17/09				
CB-2.2	7/15/05			TC-41.20	1/19/01	TC-82.10	4/19/02				
CB-4.2	7/19/02			TC-41.40	7/16/04	TC-83.10	1/19/07				
				TC-41.41	1/19/01	TC-84.20	1/19/07				
				TC-42.10	1/19/07	TC-84.21	1/19/07				
				TC-42.20	7/16/04	TC-85.10	4/19/02				
				TC-51.11	4/20/01	TC-85.20	3/06/09				

SPECIAL PROVISIONS
NWP#14 6/19/09

I:\HIGHWAYS\Data\CTID\B2140\roadway\sheet\B2140GT001.dgn 7/14/2009 3:46:14 PM jdelaney

FEDERAL PROJECT NO. E070489
PID NO. 82140
CONSTRUCTION PROJECT NO.
RAILROAD INVOLVEMENT
CLE-28-2.05



0 100 200
HORIZONTAL SCALE IN FEET

CALCULATED GWA CHECKED JMD

SCHEMATIC PLAN

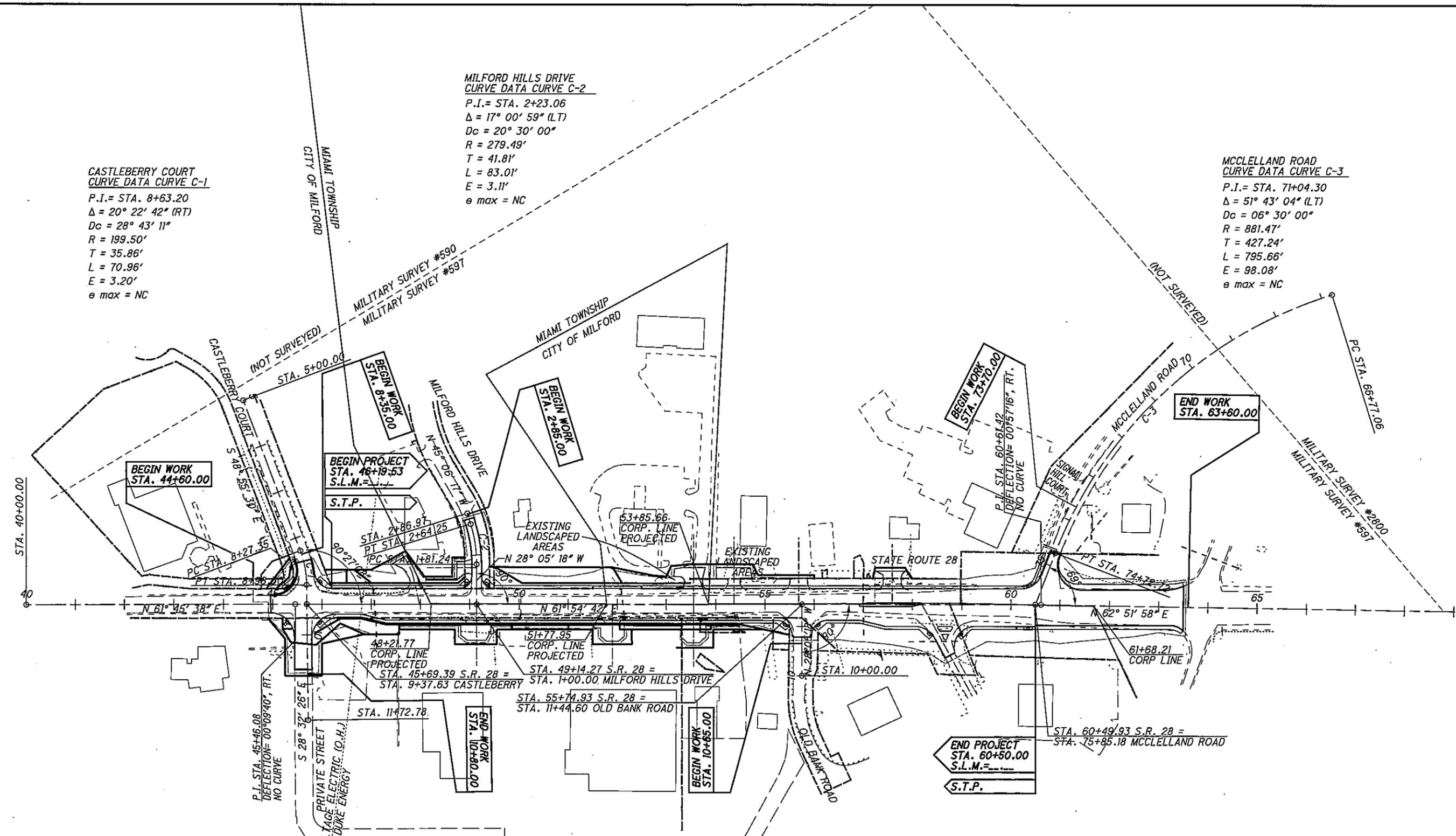
CLE-28-2.05

2

**CASTLEBERRY COURT
CURVE DATA CURVE C-1**
P.I.= STA. 8+63.20
Δ = 20° 22' 42" (RT)
Dc = 28° 43' 11"
R = 199.50'
T = 35.86'
L = 70.96'
E = 3.20'
e max = NC

**MILFORD HILLS DRIVE
CURVE DATA CURVE C-2**
P.I.= STA. 2+23.06
Δ = 17° 00' 59" (LT)
Dc = 20° 30' 00"
R = 279.49'
T = 41.81'
L = 83.01'
E = 3.11'
e max = NC

**MCCLELLAND ROAD
CURVE DATA CURVE C-3**
P.I.= STA. 71+04.30
Δ = 51° 43' 04" (LT)
Dc = 06° 30' 00"
R = 881.47'
T = 427.24'
L = 795.66'
E = 98.08'
e max = NC



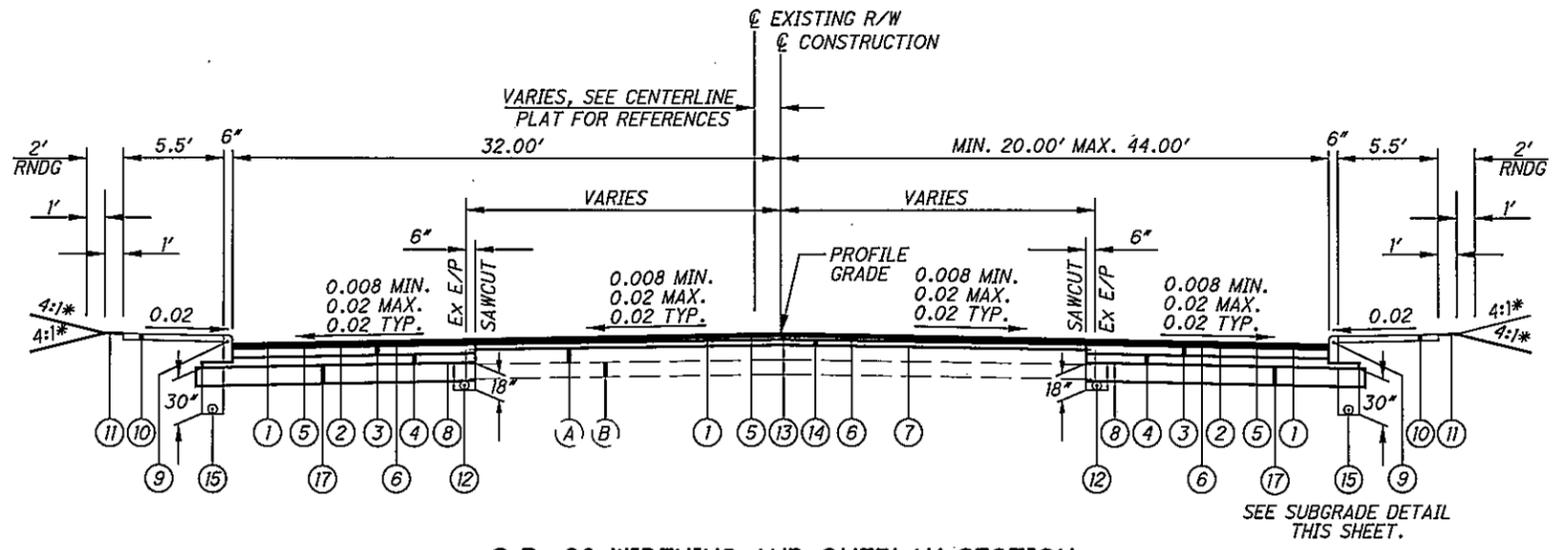
DESIGN DESIGNATION

	S.R. 28	CASTLEBERRY CT./PRIVATE STREET	MILFORD HILLS DR.	OLD BANK RD.	MCCLELLAND RD.
CURRENT ADT (2011)	28940	2920	460	1060	5550
DESIGN YEAR ADT (2031)	30950	2920	460	1060	5550
DESIGN HOURLY VOLUME (2031)	3095	204	51	95	500
DIRECTIONAL DISTRIBUTION	0.51	0.62	0.60	0.70	0.54
TRUCKS (24 HOUR B&C)	0.02	0.04	0.04	0.04	0.04
DESIGN SPEED	40 MPH	30 MPH	30 MPH	30 MPH	40 MPH
LEGAL SPEED	35 MPH	25 MPH	25 MPH	25 MPH	35 MPH
DESIGN FUNCTIONAL CLASSIFICATION:	URBAN MINOR ARTERIAL	URBAN LOCAL	URBAN LOCAL	URBAN LOCAL	URBAN LOCAL

NHS PROJECT ----- N/A

I:\highways\dot\cctf\82140\roadway\sheet\821406B001.dgn 7/14/2009 7:12:27 AM jdelaney

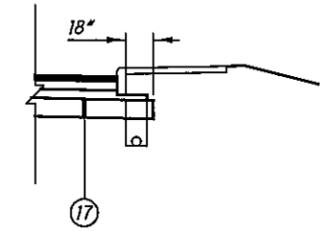
I:\highways\dat\cc\fig\82140\roadway\sheet\82140G\Y001.dgn 7/14/2009 7:12:32 AM jdelaney



S.R. 28 WIDENING AND OVERLAY SECTION

STA. 44+96.21 TO STA. 61+44.71, LT.
STA. 44+96.21 TO STA. 60+50.00, RT.

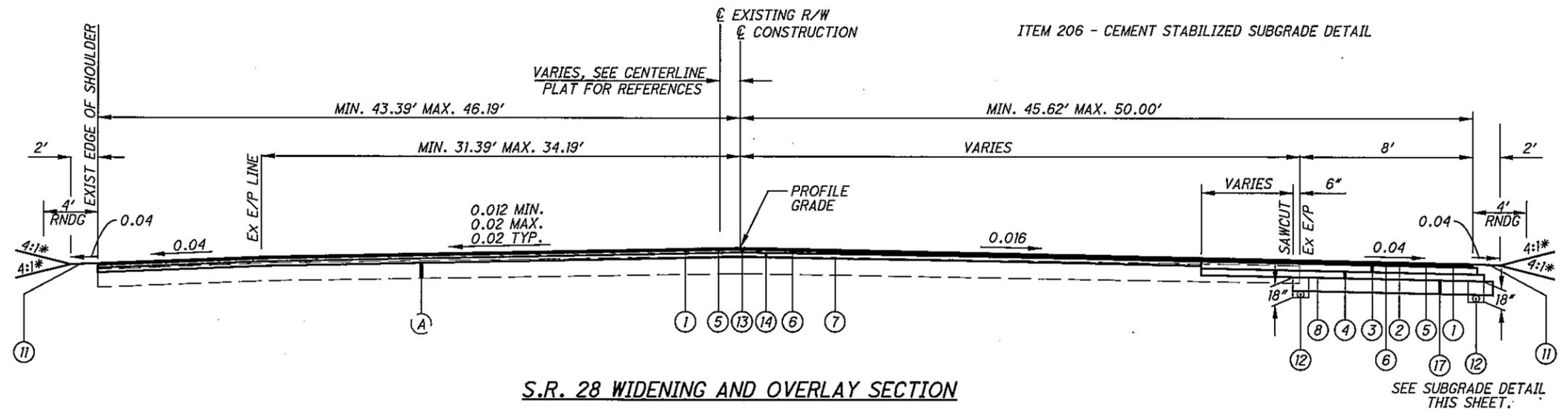
FOR RAISED MEDIAN DETAIL, SEE SHEET



ITEM 206 - CEMENT STABILIZED SUBGRADE DETAIL

- LEGEND**
- ① ITEM 448 - 1/2" ASPHALT CONCRETE, SURFACE COURSE, TYPE 1, PG 64-22
 - ② ITEM 448 - 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG 64-22
 - ③ ITEM 301 - 6" ASPHALT CONCRETE BASE, PG 64-22
 - ④ ITEM 304 - 6" AGGREGATE BASE
 - ⑤ ITEM 407 - TACK COAT FOR INTERMEDIATE COURSE
 - ⑥ ITEM 407 - TACK COAT
 - ⑦ ITEM 254 - 1/2" PAVEMENT PLANING, ASPHALT CONCRETE
 - ⑧ ITEM 204 - PROOF ROLLING
 - ⑨ ITEM 609 - CURB, TYPE 6
 - ⑩ ITEM 608 - 4" CONCRETE WALK
 - ⑪ ITEM 659 - SEEDING AND MULCHING
 - ⑫ ITEM 605 - 6" BASE PIPE UNDERDRAIN
 - ⑬ ITEM 448 - VARIABLE THICKNESS ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG 64-22
 - ⑭ ITEM 301 - VARIABLE THICKNESS ASPHALT CONCRETE BASE, PG 64-22
 - ⑮ ITEM 605 - 6" SHALLOW PIPE UNDERDRAIN
 - ⑯ ITEM 609 - CONCRETE MEDIAN
 - ⑰ ITEM 206 - CEMENT STABILIZED SUBGRADE, 12 INCHES DEEP
 - A EXISTING PAVEMENT: 0.8' ASPHALT FROM 44+96.21 TO 52+00
1.3' ASPHALT FROM 52+00 TO 63+50
(SEE SOIL PROFILES FOR ADDITIONAL DETAIL)
 - B EXISTING PAVEMENT: 0.8' CONCRETE FROM 44+96.21 TO 52+00
0.0' CONCRETE FROM 52+00 TO 63+50
(SEE SOIL PROFILES FOR ADDITIONAL DETAIL)
- * OR AS SHOWN IN THE CROSS SECTIONS

NOTE: SIDE ROADS SHALL HAVE THE SAME BUILD UP AS THE ROADWAY PAVEMENT UNLESS OTHERWISE NOTED.



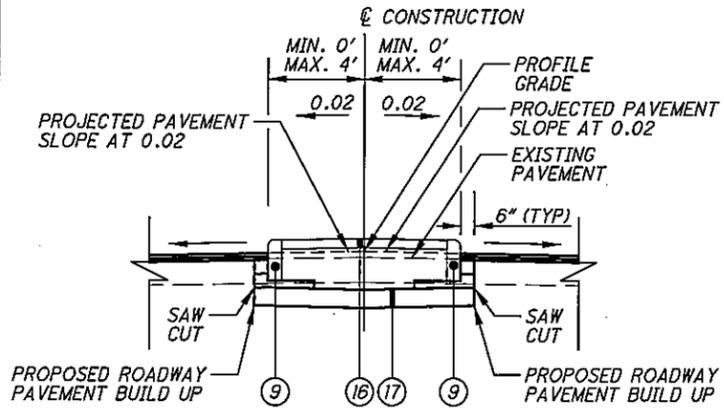
S.R. 28 WIDENING AND OVERLAY SECTION

STA. 61+44.71 TO STA. 63+50.00, LT.
STA. 60+50.00 TO STA. 63+50.00, RT.

SEE PAVEMENT ELEVATION TABLE FOR CROSS SLOPE, SHEET

TYPICAL SECTIONS

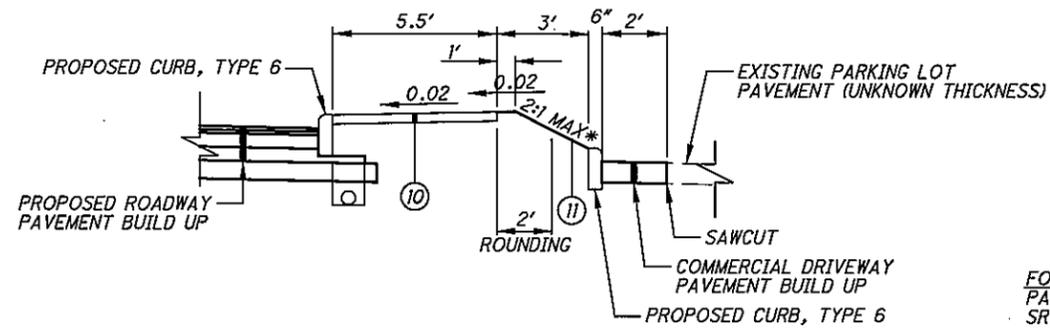
CLE-28-2.05



RAISED MEDIAN DETAIL

NTS

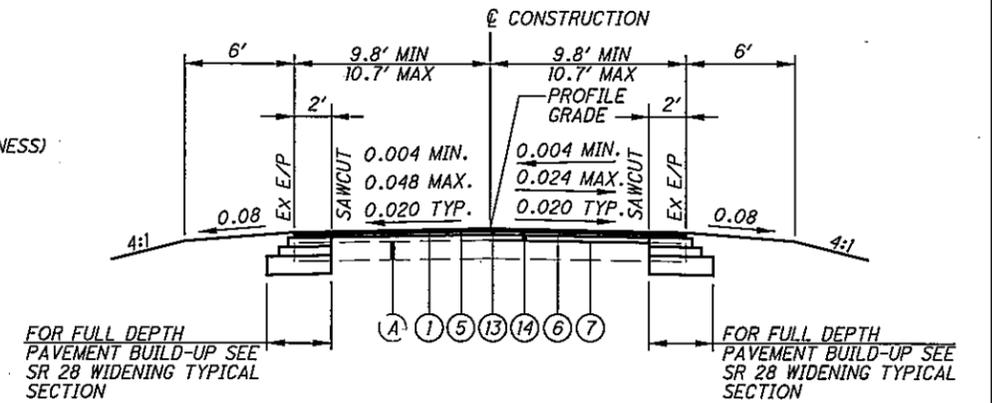
STA. 56+92.24 TO STA. 58+17.91



CURB, TYPE 6 AT PARKING LOT DETAIL

NTS

STA. 47+56.34 RT. TO STA. 54+47.47 RT.



**MILFORD HILLS DRIVE
WIDENING AND OVERLAY SECTION**

STA. 1+57.00 TO STA. 2+75.00

FOR CROSS SLOPE LIMITS AND ELEVATIONS
SEE INTERSECTION DETAIL SHEET

* OR AS SHOWN IN THE CROSS SECTIONS
FOR PAVEMENT BUILD-UP
LEGEND, SEE SHEET 3

i:\highways\dat\cc\fid\82140\roadway\sheet\82140GY002.dgn 7/14/2009 6:15:51 AM jdelaney

ROUNDING
THE ROUNDING AT SLOPE BREAKPOINTS SHOWN ON THE TYPICAL SECTIONS APPLY TO ALL CROSS-SECTIONS EVEN THOUGH OTHERWISE SHOWN.

UTILITIES
LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

WATER:
CLERMONT COUNTY SEWER DISTRICT
2379 CLERMONT CENTER DRIVE
BATAVIA, OHIO 45103
LYLE BLOOM
513-732-8860

CITY OF MILFORD
745 CENTER STREET
MILFORD, OHIO 45150
MATTHEW NEWMAN
513-831-6819

SANTIARY:
CITY OF MILFORD
745 CENTER STREET
MILFORD, OHIO 45150
BUD WHITE
513-248-5098

TELECOMMUNICATIONS:
CINCINNATI BELL TELEPHONE
221 EAST FOURTH STREET
BUILDING 343
CINCINNATI, OHIO 45201-2301
MARK CONNOR
513-565-7043

GAS:
DUKE ENERGY
139 EAST FOURTH STREET
ROOM 460A
CINCINNATI, OHIO 45202
BILL HOCTOR
513-287-2916

ELECTRIC:
DUKE ENERGY
139 EAST FOURTH STREET
ROOM 467 ANNEX
CINCINNATI, OHIO 45201
VIRGINIA MEYER
513-287-1748

TIME WARNER CABLE
11252 CORNELL PARK DRIVE
CINCINNATI, OHIO 45202
GARY NAPIER
513-469-5483

ODOT DISTRICT 8
505 SOUTH SR 741
LEBANON, OHIO 45036
JIM JUDD
513-933-6692

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

ELEVATION DATUM
ALL ELEVATIONS ARE ORTHOMETRIC HEIGHTS USING THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88) AND THE GEOID03 GEOID. HORIZONTAL POSITIONS ARE BASED ON THE OHIO STATE PLANE SOUTH ZONE, A LAMBERT CONFORMAL CONIC MAP PROJECTION, THE NORTH AMERICAN DATUM OF 1983 ADJUSTED TO THE NATIONAL SPATIAL REFERENCE SYSTEM OF 2007 (NAD 83/NSRS 2007), AND THE WGS84 ELLIPSOID.

WORK LIMITS
THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

CLEARING AND GRUBBING
REMOVE ALL TREES AND STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE CONSTRUCTION LIMITS UNDER THE LUMP SUM BID FOR ITEM 201, CLEARING AND GRUBBING. THE FOLLOWING IS AN APPROXIMATE ESTIMATE OF THE NUMBER OF TREES AND STUMPS TO BE REMOVED.

SIZES	NO. TREES	NO. STUMPS	TOTAL
18"			
30"			
48"			
60"			

PART-WIDTH CONSTRUCTION
BECAUSE OF THE NECESSITY TO BUILD THIS PROJECT UNDER TRAFFIC AND TO CONSTRUCT THE FULL PAVEMENT WIDTH IN STAGES, EXERCISE CARE TO PREVENT THE CONSTRUCTION OF A BUTT JOINT IN THE BASE COURSES. LAP LONGITUDINAL JOINTS AS SHOWN ON STANDARD CONSTRUCTION DRAWING BP-3.1.

PAVEMENT RESTORATION FOR PIPE INSTALLATIONS
THE FOLLOWING QUANTITY HAS BEEN PROVIDED FOR PAVEMENT RESTORATION FOLLOWING INSTALLATION OF PIPES UNDER ITEM 603.

ITEM 301 ASPHALT CONCRETE BASE, PG64-22 ----- CU. YDS.

THE ABOVE QUANTITY IS BASED ON A 301 THICKNESS OF 12 INCHES AND A PAVEMENT RESTORATION WIDTH THAT INCLUDES THE TRENCH WIDTH PLUS TWO FEET ON EACH SIDE OF THE TRENCH. SEE STANDARD CONSTRUCTION DRAWING DM-1.4 FOR TRENCH WIDTH FORMULA AND CALCULATION.

PROVIDE ANY MATERIALS USED OUTSIDE THE LIMITS STATED ABOVE AT NO ADDITIONAL COST.

ITEM SPECIAL - MAILBOX SUPPORT
THIS WORK SHALL CONSIST OF FURNISHING AND ERECTING MAILBOX SUPPORTS AND ANY ASSOCIATED MOUNTING HARDWARE IN ACCORDANCE WITH PLAN DETAILS, AND ATTACHING AN OWNER-SUPPLIED MAILBOX AT LOCATIONS SPECIFIED IN THE PLAN, OR OTHERWISE ESTABLISHED BY THE ENGINEER.

WOOD POSTS SHALL BE NOMINAL 4" BY 4" SQUARE OR 4 1/2" DIAMETER ROUND, AND CONFORM TO 710.14.

STEEL POSTS SHALL BE NOMINAL PIPE SIZE 2" I.D. O.D. , AND CONFORM TO AASHTO M 181.

HARDWARE (PLATES, SCREWS, BOLTS, ETC.) SHALL BE COMMERCIAL-GRADE GALVANIZED STEEL.

POSTS SHALL BE SET PER THE FIRST PARAGRAPH OF 606.03, AND SHALL IN NO INSTANCE BE ENCASED IN CONCRETE.

SUPPORT HARDWARE SHALL ACCOMMODATE EITHER A SINGLE OR A DOUBLE MAILBOX INSTALLATION, AND NO MORE THAN TWO BOXES MAY BE MOUNTED ON A SINGLE POST.

THE MAILBOX SHALL BE SECURELY AND NEATLY ATTACHED BY THE CONTRACTOR TO THE NEW SUPPORT. THE CONTRACTOR SHALL FURNISH ALL NECESSARY ATTACHMENT HARDWARE (NUTS, BOLTS, PLATES, SPACERS, AND WASHERS) AS NECESSARY TO ACCOMMODATE THE COMPLETE INSTALLATION.

IN THE ABSENCE OF A NEW BOX SUPPLIED BY THE OWNER, THE CONTRACTOR SHALL SALVAGE THE EXISTING BOX AND PLACE IT ON THE NEW SUPPORT. DUE CARE SHALL BE EXERCISED IN SUCH AN OPERATION, AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING ANY BOX DAMAGED BY IMPROPER HANDLING ON HIS PART, AS JUDGED AND DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE LOCAL POST MASTER REGARDING THE TIMING OF THE MOVEMENT OF ANY MAILBOX TO A NEW LOCATION.

PAYMENT UNDER THIS ITEM SHALL BE LIMITED TO FINAL PERMANENT INSTALLATIONS. TEMPORARY INSTALLATIONS SHALL BE IN ACCORDANCE WITH 107.10. HOWEVER, THE SAME MATERIAL AND SIZE LIMITATIONS AS FOR PERMANENT INSTALLATIONS SHALL APPLY.

MAILBOX SUPPORTS, COMPLETE IN PLACE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH, FOR ITEM SPECIAL MAILBOX SUPPORT SYSTEM, (SINGLE) (DOUBLE).

CROSSINGS AND CONNECTIONS TO EXISTING PIPES AND UTILITIES
WHERE PLANS PROVIDE FOR A PROPOSED CONDUIT TO BE CONNECTED TO, OR CROSS OVER OR UNDER AN EXISTING SEWER OR UNDERGROUND UTILITY, THE CONTRACTOR SHALL LOCATE THE EXISTING PIPES OR UTILITIES BOTH AS TO LINE AND GRADE BEFORE STARTING TO LAY THE PROPOSED CONDUIT.

IF IT IS DETERMINED THAT THE ELEVATION OF THE EXISTING CONDUIT, OR EXISTING APPURTENANCE TO BE CONNECTED, DIFFERS FROM THE PLAN ELEVATION OR RESULTS IN A CHANGE IN THE PLAN CONDUIT SLOPE, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WILL BE AFFECTED BY THE VARIANCE IN THE EXISTING ELEVATIONS.

IF IT IS DETERMINED THAT THE PROPOSED CONDUIT WILL INTERSECT AN EXISTING SEWER OR UNDERGROUND UTILITY IF CONSTRUCTED AS SHOWN ON THE PLAN, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WOULD BE AFFECTED BY THE INTERFERENCE WITH AN EXISTING FACILITY.

PAYMENT FOR ALL THE OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 603 CONDUIT ITEM.

REVIEW OF DRAINAGE FACILITIES
BEFORE ANY WORK IS STARTED ON THE PROJECT AND AGAIN BEFORE FINAL ACCEPTANCE BY THE STATE, REPRESENTATIVES OF THE STATE AND THE CONTRACTOR, ALONG WITH LOCAL REPRESENTATIVES, SHALL MAKE AN INSPECTION OF ALL EXISTING SEWERS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCE SHALL BE DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION SHALL BE KEPT IN WRITING BY THE STATE.

ALL NEW CONDUITS, INLETS, CATCH BASINS, AND MANHOLES CONSTRUCTED AS A PART OF THE PROJECT SHALL BE FREE OF ALL FOREIGN MATTER AND IN A CLEAN CONDITION BEFORE THE PROJECT WILL BE ACCEPTED BY THE STATE.

ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE MENTIONED PARTIES SHALL BE MAINTAINED AND LEFT IN A CONDITION REASONABLY COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. ANY CHANGE IN THE CONDITION RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 603 CONDUIT ITEMS.

MANHOLES, CATCH BASINS AND INLETS REMOVED OR ABANDONED
ALL CASTINGS SHALL BE CAREFULLY REMOVED AND STORED WITHIN THE RIGHT OF WAY FOR SALVAGE BY COUNTY FORCES.

PAYMENT FOR ALL OF THE ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 202 ITEM.

SEEDING AND MULCHING
THE FOLLOWING QUANTITIES ARE PROVIDED TO PROMOTE GROWTH AND CARE OF PERMANENT SEEDING AREAS:

659, SOIL ANALYSIS TEST	----	EACH
659, TOPSOIL	----	CU. YD.
659, SEEDING AND MULCHING	----	SO. YD.
659, REPAIR SEEDING AND MULCHING	----	SO. YD.
659, INTER-SEEDING	----	SO. YD.
659, COMMERCIAL FERTILIZER	----	TON
659, LIME	----	ACRES
659, WATER	----	M. GAL.
659, MOWING	----	M. SQ. FT.

SEEDING AND MULCHING SHALL BE APPLIED TO ALL AREAS OF EXPOSED SOIL BETWEEN THE RIGHT-OF-WAY LINES, AND WITHIN THE CONSTRUCTION LIMITS FOR AREAS OUTSIDE THE RIGHT-OF-WAY LINES COVERED BY WORK AGREEMENT OR SLOPE EASEMENT. QUANTITY CALCULATIONS FOR SEEDING AND MULCHING ARE BASED ON THESE LIMITS.

ITEM 407 - TACK COAT
THE RATE OF APPLICATION OF THE 407 TACK COAT SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. FOR ESTIMATING PURPOSES ONLY, THE PLAN QUANTITIES INDICATE AN AVERAGE APPLICATION RATE OF:

ITEM 407 - TACK COAT FOR INTERMEDIATE COURSE	0.04 GAL PER SQ YD
ITEM 407 - TACK COAT	0.075 GAL PER SQ YD

ITEM 206 - CURING COMPOUND, AS PER PLAN
CURE THE CEMENT STABILIZED SUBGRADE WITH RAPID SETTING EMULSIFIED ASPHALT, CONFORMING TO 702.04. NO SUBSTITUTE FOR THE EMULSIFIED ASPHALT CURE SHALL BE PERMITTED. ALL OTHER ITEMS OF ITEM 206, CEMENT STABILIZED SUBGRADE SHALL APPLY.

CALCULATED
GKB
CHECKED
AJS

GENERAL NOTES

CLE-28-2.05

5

SEQUENCE OF CONSTRUCTION

GENERAL REQUIREMENTS

BEFORE THE COMMENCEMENT OF EACH CONSTRUCTION PHASE THE CONTRACTOR SHALL REVISE THE PAVEMENT MARKINGS, REMOVE CONFLICTING PAVEMENT MARKINGS, AND COVER CONFLICTING SIGNS.

PRE-CONSTRUCTION PHASE (NOT SHOWN)

CONSTRUCT THE PROPOSED DRAINAGE SYSTEM AND ASSOCIATED GRADING. MILL EXISTING PAVEMENT BETWEEN STA. 47+00 AND STA. 52+00, AND RESURFACE TO THE BOTTOM OF THE INTERMEDIATE COURSE. PAVING SHALL BE PERFORMED UNDER TRAFFIC USING FLAGGERS PER MT-97.12. IF PAVEMENT BUILDUP IS NOT COMPLETED IN ONE DAY, UTILIZE TEMPORARY PAVEMENT MARKINGS.

PHASE 1

PLACE TEMPORARY PAVEMENT MARKINGS AND MAINTAIN 3-10 FT LANES ALONG STATE ROUTE 28 AS SHOWN IN "PHASE 1". CONSTRUCT THE PAVEMENT WIDENING ALONG THE NORTH SIDE OF STATE ROUTE 28 TO THE TOP OF ITEM 301 FROM STATION 46+20 TO INTERCHANGE 275. PLACE EXISTING PAVEMENT WHERE NEEDED TO MAINTAIN A FLUSH CONDITION BETWEEN EXISTING PAVEMENT AND NEWLY CONSTRUCTED PAVEMENT. MAINTAIN ACCESS TO BUSINESSES USING ITEM 410. CONTRACTOR SHALL COMPLY WITH DROP OFFS IN WORK ZONE CRITERIA FOR NIGHT TIME USE OF DRUMS.

CASTLEBERRY COURT INTERSECTION - WEST: 1. CONSTRUCT THE RETURN RADII UNDER TRAFFIC. 2. REVISE THE SIGNAL TO BLINKING RED/YELLOW. CONSTRUCT FULL DEPTH PAVEMENT TO THE TOP OF THE INTERMEDIATE COURSE UNDER TRAFFIC UTILIZING FLAGGERS. MAINTAIN ONE LANE DURING DAYLIGHT HOURS. DURING NIGHT TIME HOURS MAINTAIN TWO LANE ACCESS USING ITEM 410.

MCCLELLAND ROAD INTERSECTION: 1. CONSTRUCT THE RETURN RADII UNDER TRAFFIC. 2. PERFORM THE RESURFACING AND WIDENING OF INTERSECTION UNDER TRAFFIC UTILIZING FLAGGERS.

MILFORD HILLS DRIVE: CONSTRUCT PAVEMENT BUILDUP AND WIDENING UNDER TRAFFIC UTILIZING FLAGGERS.

PHASE 1A (NOT SHOWN)

PERFORM PLANING ON THE REMAINING EXISTING PAVEMENT. PLACE INTERMEDIATE COURSE ON THE NEWLY CONSTRUCTED PAVEMENT AND EXISTING PAVEMENT. PAVING SHALL BE PERFORMED UNDER TRAFFIC USING FLAGGERS PER MT-97.12.

PHASE 2

ROUTE TRAFFIC ALONG THE NORTHERN SIDE OF STATE ROUTE 28 ON PROPOSED AND EXISTING PAVEMENT, MAINTAIN 3-11 FT LANES. CONSTRUCT THE PAVEMENT WIDENING ALONG THE SOUTH SIDE OF STATE ROUTE 28 TO THE TOP OF THE INTERMEDIATE COURSE AS SHOWN IN "PHASE 2". MAINTAIN ACCESS TO BUSINESSES USING ITEM 410. CONTRACTOR SHALL COMPLY WITH DROP ZONE CRITERIA FOR NIGHT TIME USE OF DRUMS.

CASTLEBERRY COURT INTERSECTION - EAST: 1. CONSTRUCT THE RETURN RADII UNDER TRAFFIC. 2. CONSTRUCT FULL DEPTH PAVEMENT TO THE TOP OF THE INTERMEDIATE COURSE UNDER TRAFFIC UTILIZING FLAGGERS. MAINTAIN ONE LANE DURING DAYLIGHT HOURS. DURING NIGHT TIME HOURS MAINTAIN TWO LANE ACCESS USING ITEM 410.

OLD BANK ROAD: 1. CONSTRUCT THE RETURN RADII UNDER TRAFFIC. 2. PERFORM THE RESURFACING AND WIDENING OF INTERSECTION UNDER TRAFFIC UTILIZING FLAGGERS.

PHASE 3

REVISE PAVEMENT MARKINGS. CONSTRUCT THE RAISED MEDIAN FROM STA. 56+92.00 TO STA. 58+18.00 AS SHOWN IN "PHASE 3". PLACE SURFACE COURSE. PAVING SHALL BE PERFORMED UNDER TRAFFIC USING FLAGGERS PER MT-97.12. PLACE FINAL PAVEMENT MARKING.

CALCULATED
TAM
CHECKED
MAG

MAINTENANCE OF TRAFFIC GENERAL NOTES

CLE-28-2.05



COMMERCIAL VACANT LAND

MILFORD HILLS DRIVE

DRIVE CURVE DATA
 P.I. STA. 10+71.21
 $\Delta = 16^\circ 54' 22''$ (LT)
 $Dc = 57' 17'' 45''$
 $R = 100.00'$
 $T = 14.86'$
 $L = 29.51'$
 $E = 1.10'$

MATCH LINE
 STA. 2+25.00 SEE SHEET 35

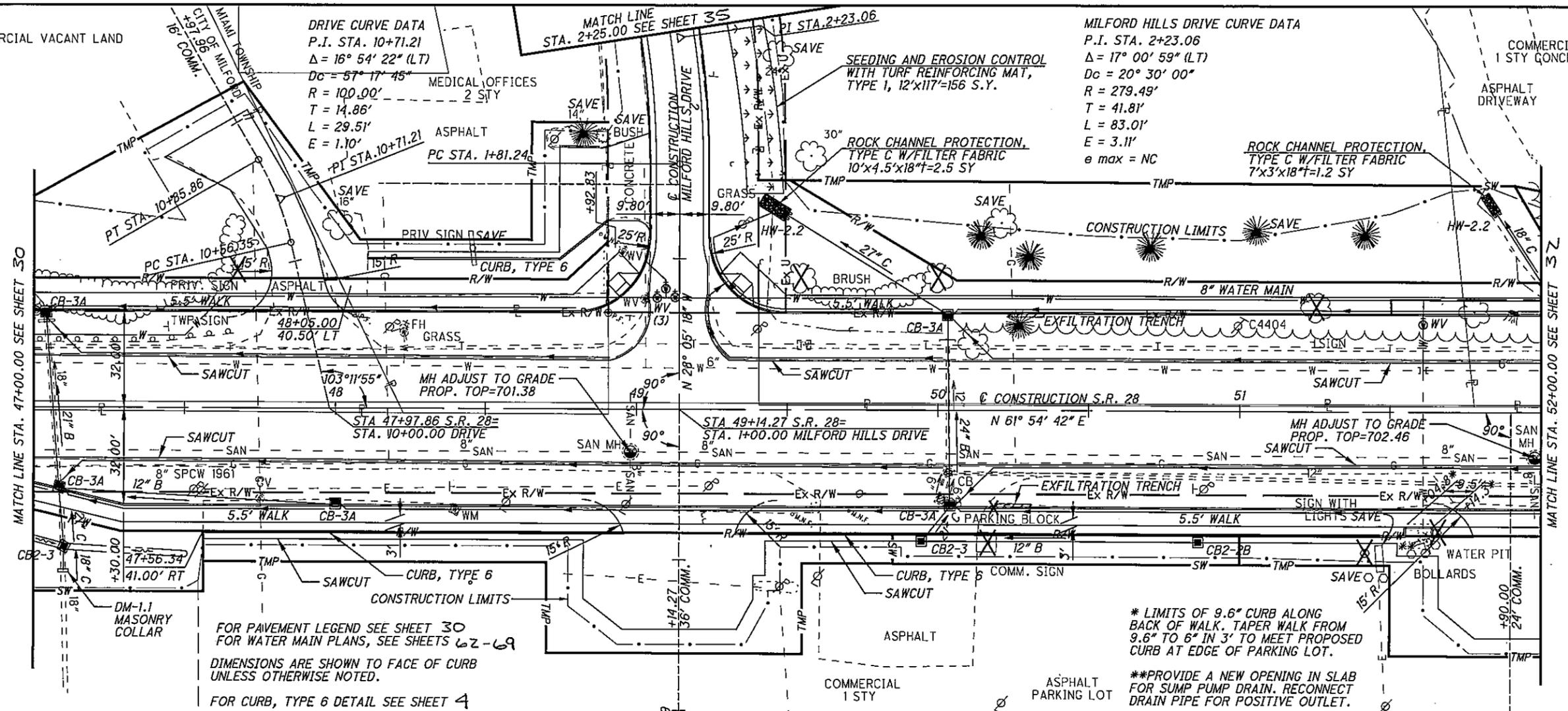
MILFORD HILLS DRIVE CURVE DATA
 P.I. STA. 2+23.06
 $\Delta = 17^\circ 00' 59''$ (LT)
 $Dc = 20^\circ 30' 00''$
 $R = 279.49'$
 $T = 41.81'$
 $L = 83.01'$
 $E = 3.11'$
 $e \text{ max} = \text{NC}$

COMMERCIAL 1 STY CONCRETE

ASPHALT DRIVEWAY

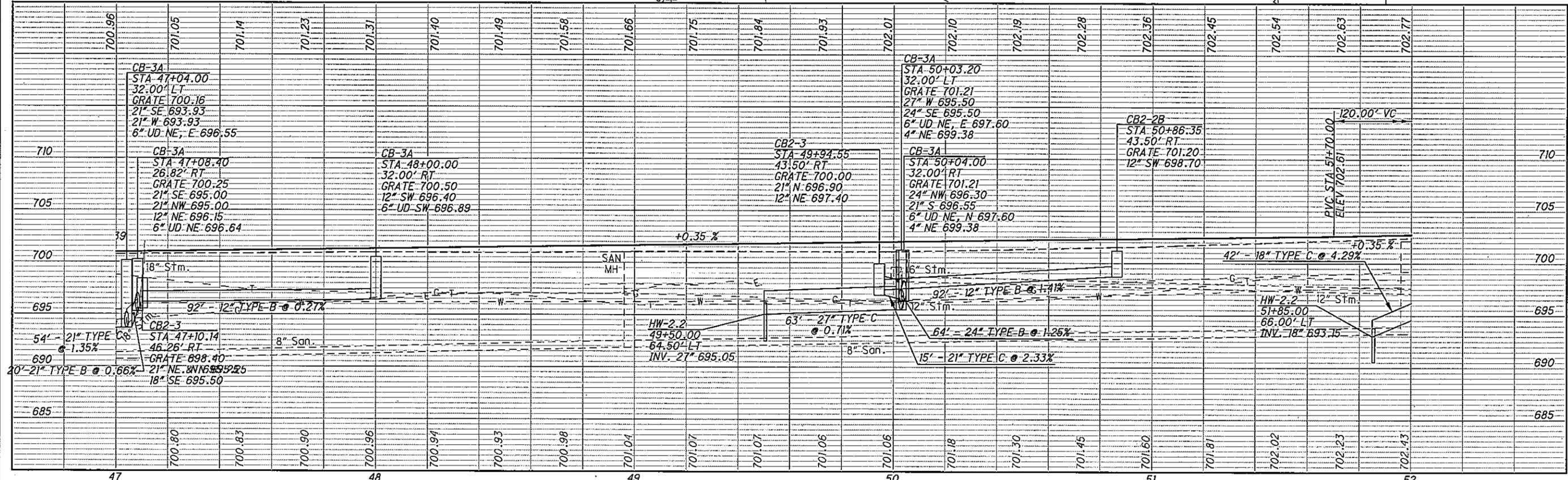
MATCH LINE STA. 47+00.00 SEE SHEET 30

MATCH LINE STA. 52+00.00 SEE SHEET 32



FOR PAVEMENT LEGEND SEE SHEET 30
 FOR WATER MAIN PLANS, SEE SHEETS 62-69
 DIMENSIONS ARE SHOWN TO FACE OF CURB
 UNLESS OTHERWISE NOTED.
 FOR CURB, TYPE 6 DETAIL SEE SHEET 4

* LIMITS OF 9.6" CURB ALONG
 BACK OF WALK. TAPER WALK FROM
 9.6" TO 6" IN 3' TO MEET PROPOSED
 CURB AT EDGE OF PARKING LOT.
 **PROVIDE A NEW OPENING IN SLAB
 FOR SUMP PUMP DRAIN. RECONNECT
 DRAIN PIPE FOR POSITIVE OUTLET.



S.R. 28 PLAN AND PROFILE
 STA. 47+00.00 TO STA. 52+00.00

CLE-28-2.05





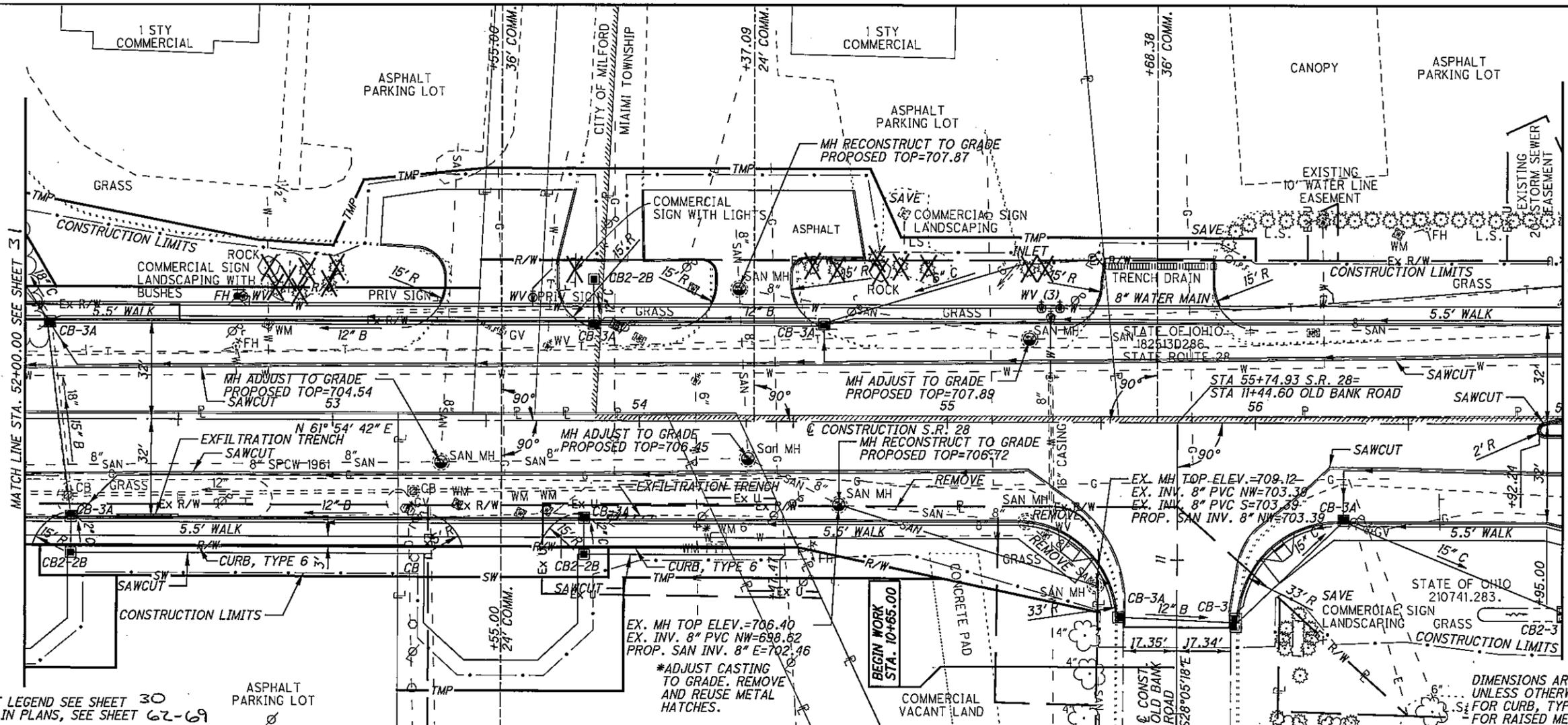
0 10 20
HORIZONTAL
SCALE IN FEET

CALCULATED
AJJ
CHECKED
GKB

S.R. 28 PLAN AND PROFILE
STA. 52+00.00 TO STA. 57+00.00

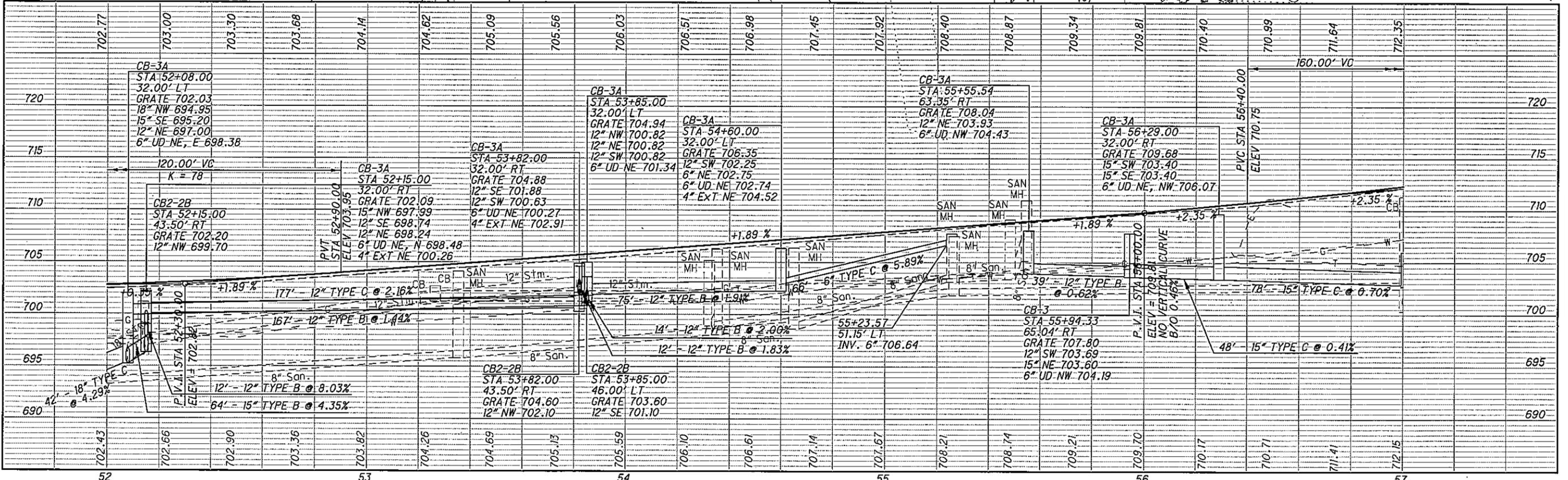
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32

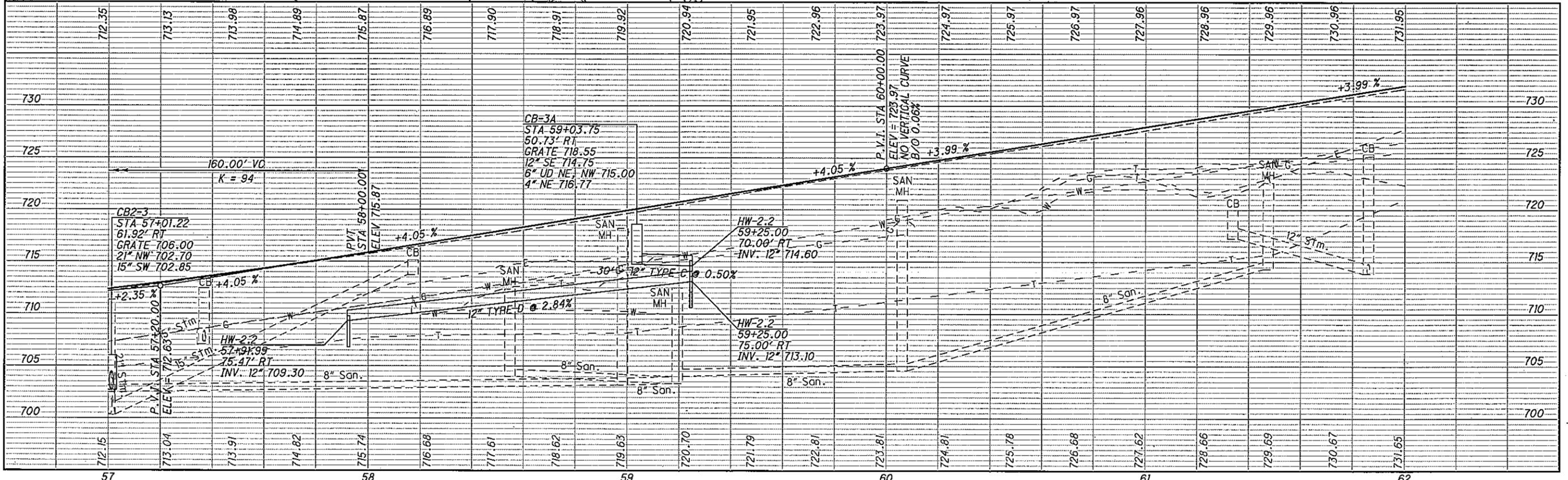
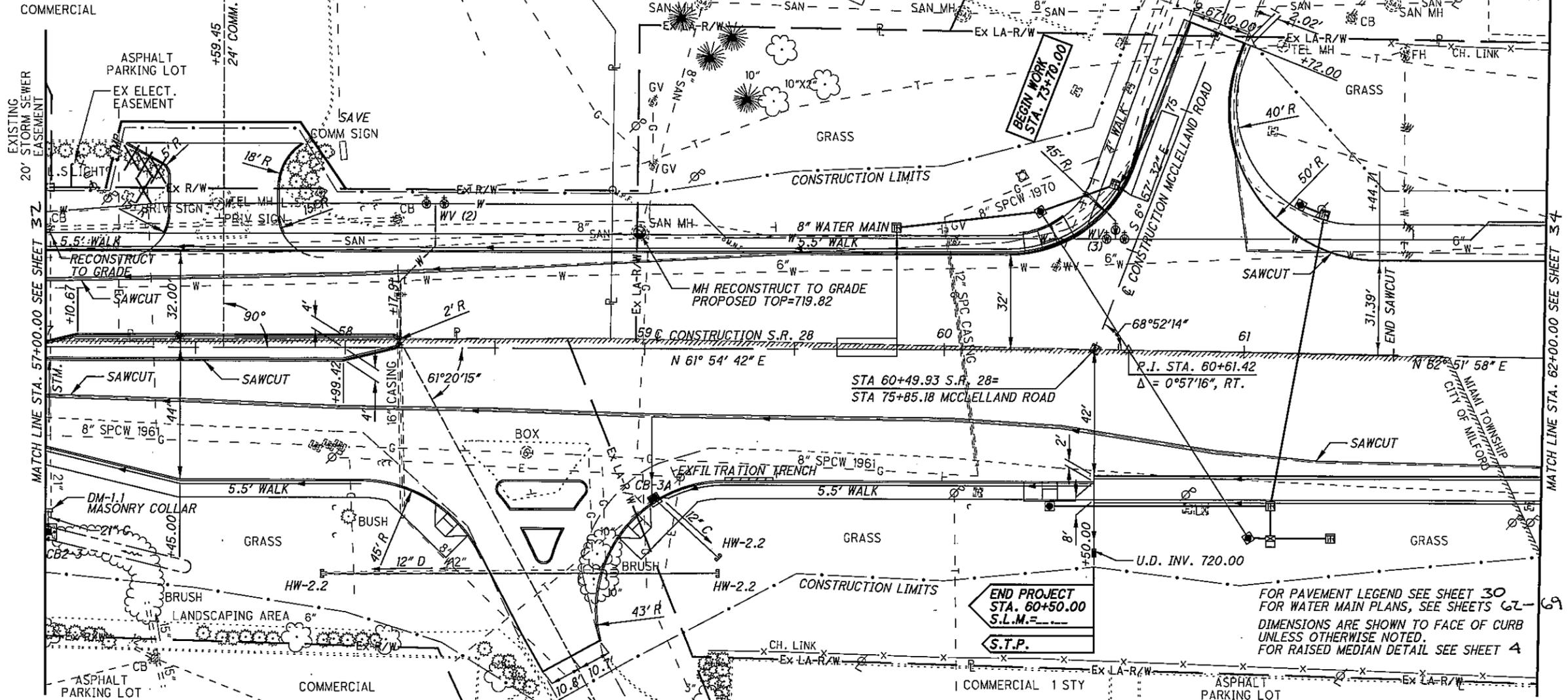


FOR PAVEMENT LEGEND SEE SHEET 30
FOR WATER MAIN PLANS, SEE SHEET 62-69

DIMENSIONS ARE SHOWN TO FACE OF CURB
UNLESS OTHERWISE NOTED.
FOR CURB, TYPE 6 DETAIL SEE SHEET 4
FOR RAISED MEDIAN DETAIL SEE SHEET 4



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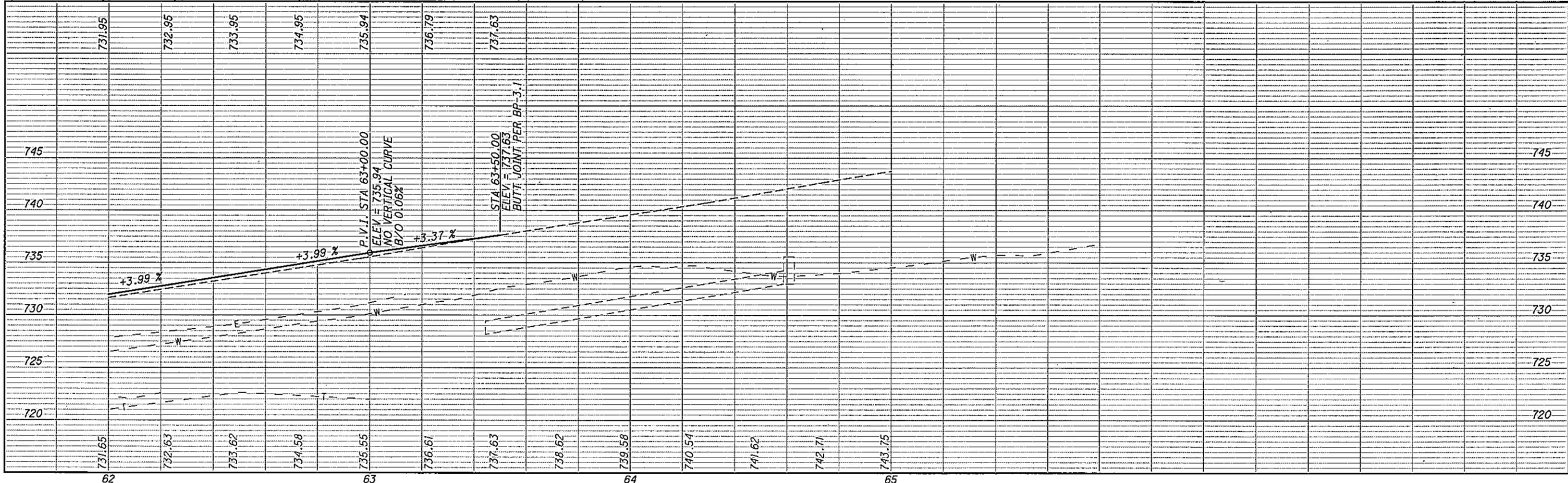
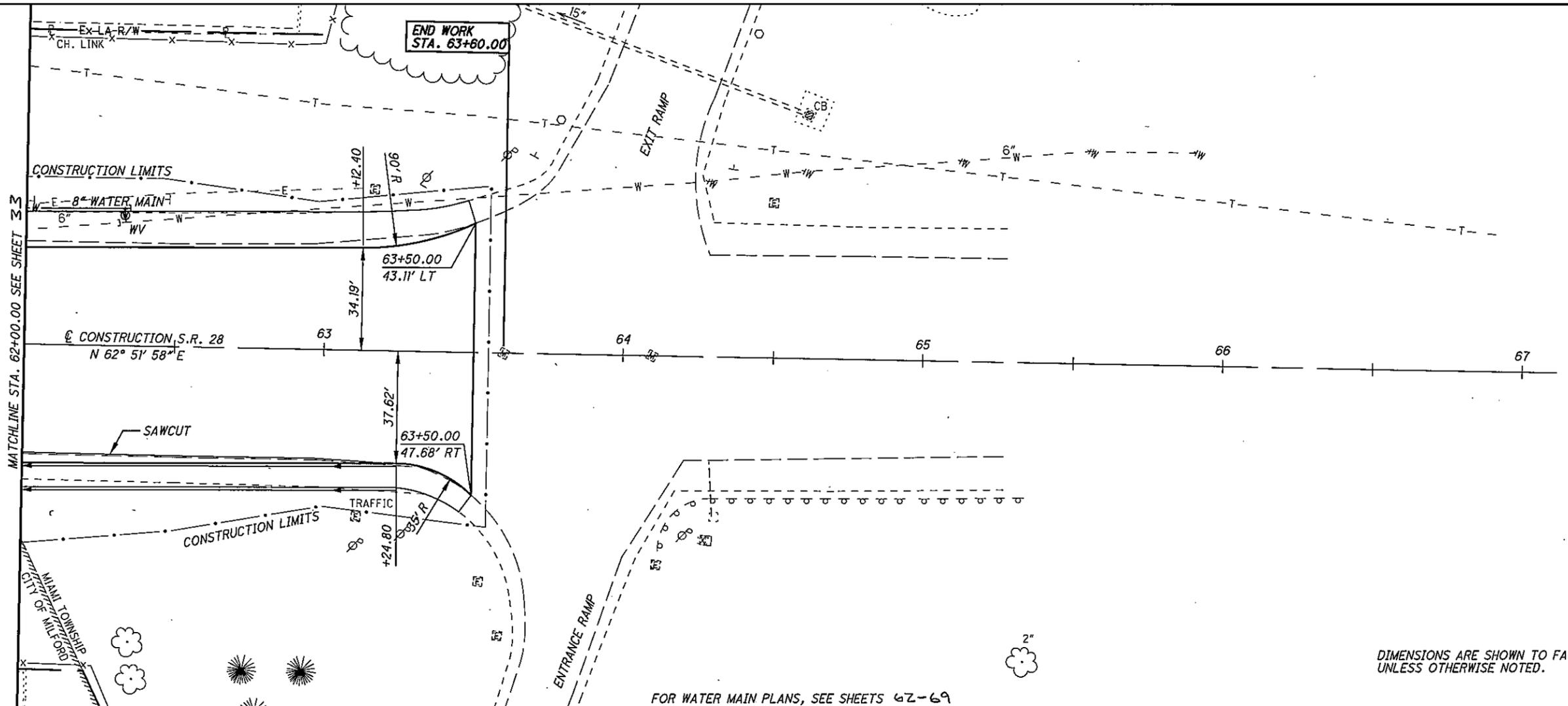




 CALCULATED: AJS
 CHECKED: GKB
S.R. 28 PLAN AND PROFILE
STA. 57+00.00 TO STA. 62+00.00
CLE-28-2.05


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CALCULATED
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CHECKED
GKB

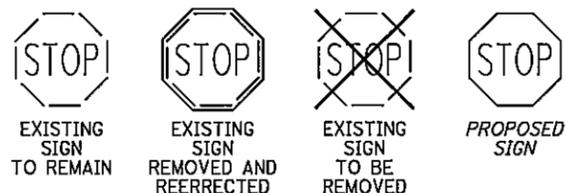
HORIZONTAL SCALE IN FEET

S.R. 28 PLAN AND PROFILE
STA. 62+00.00 TO STA. 63+50.00

CLE-28-2.05

34

SIGNING LEGEND

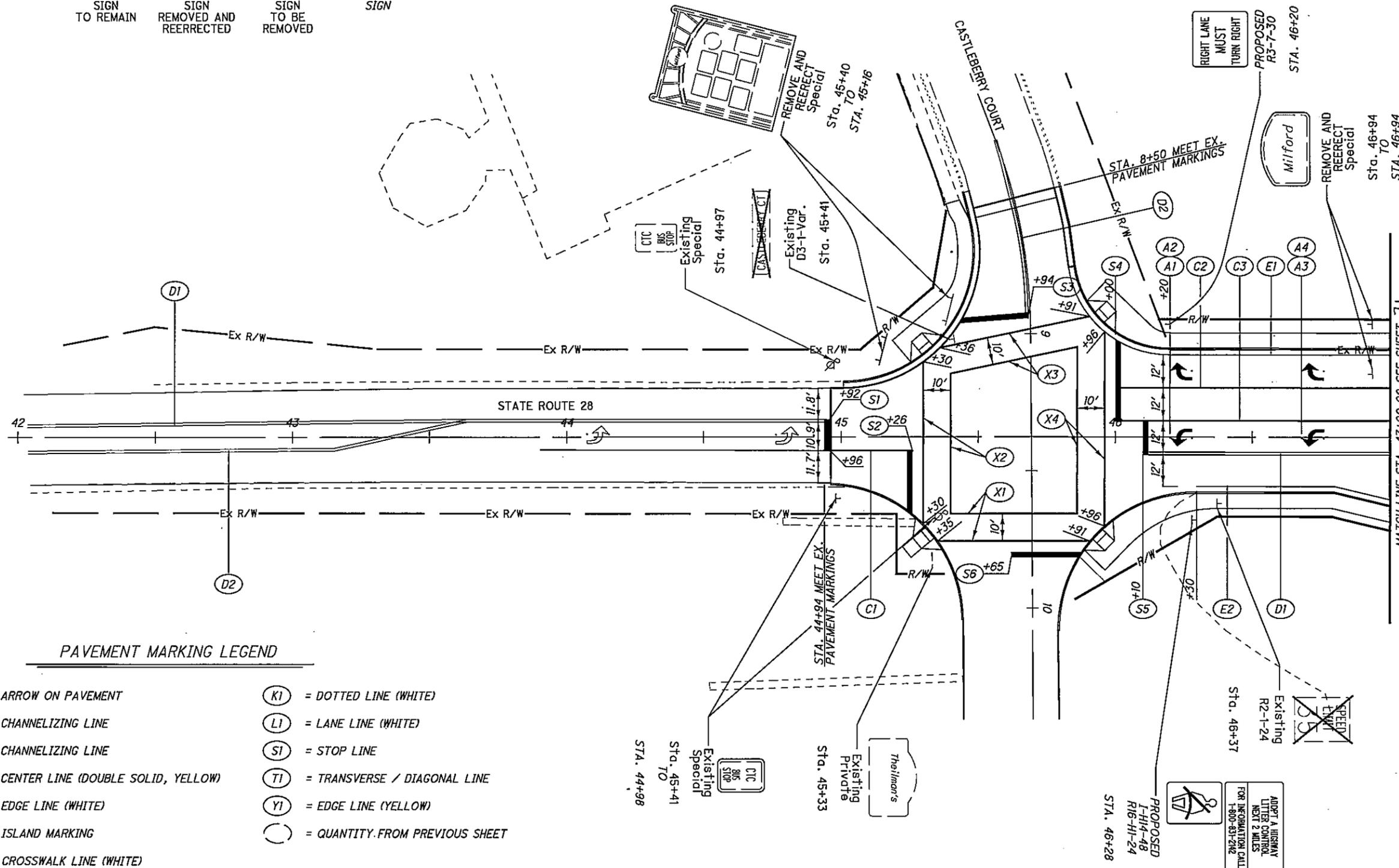


PAVEMENT MARKING LEGEND

- | | |
|---|------------------------------------|
| (A1) = ARROW ON PAVEMENT | (K1) = DOTTED LINE (WHITE) |
| (B1) = CHANNELIZING LINE | (L1) = LANE LINE (WHITE) |
| (C1) = CHANNELIZING LINE | (S1) = STOP LINE |
| (D1) = CENTER LINE (DOUBLE SOLID, YELLOW) | (T1) = TRANSVERSE / DIAGONAL LINE |
| (E1) = EDGE LINE (WHITE) | (Y1) = EDGE LINE (YELLOW) |
| (I1) = ISLAND MARKING | () = QUANTITY FROM PREVIOUS SHEET |
| (J1) = CROSSWALK LINE (WHITE) | |

NOTE :

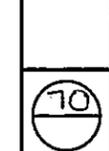
RAISED PAVEMENT MARKERS SHALL BE INSTALLED ACCORDING TO O.D.O.T. STANDARD CONSTRUCTION DRAWINGS ON ALL TYPES OF CENTER LINES, CHANNELIZING LINES AND LANE LINES.



CALCULATED
BMH
CHECKED
KRM

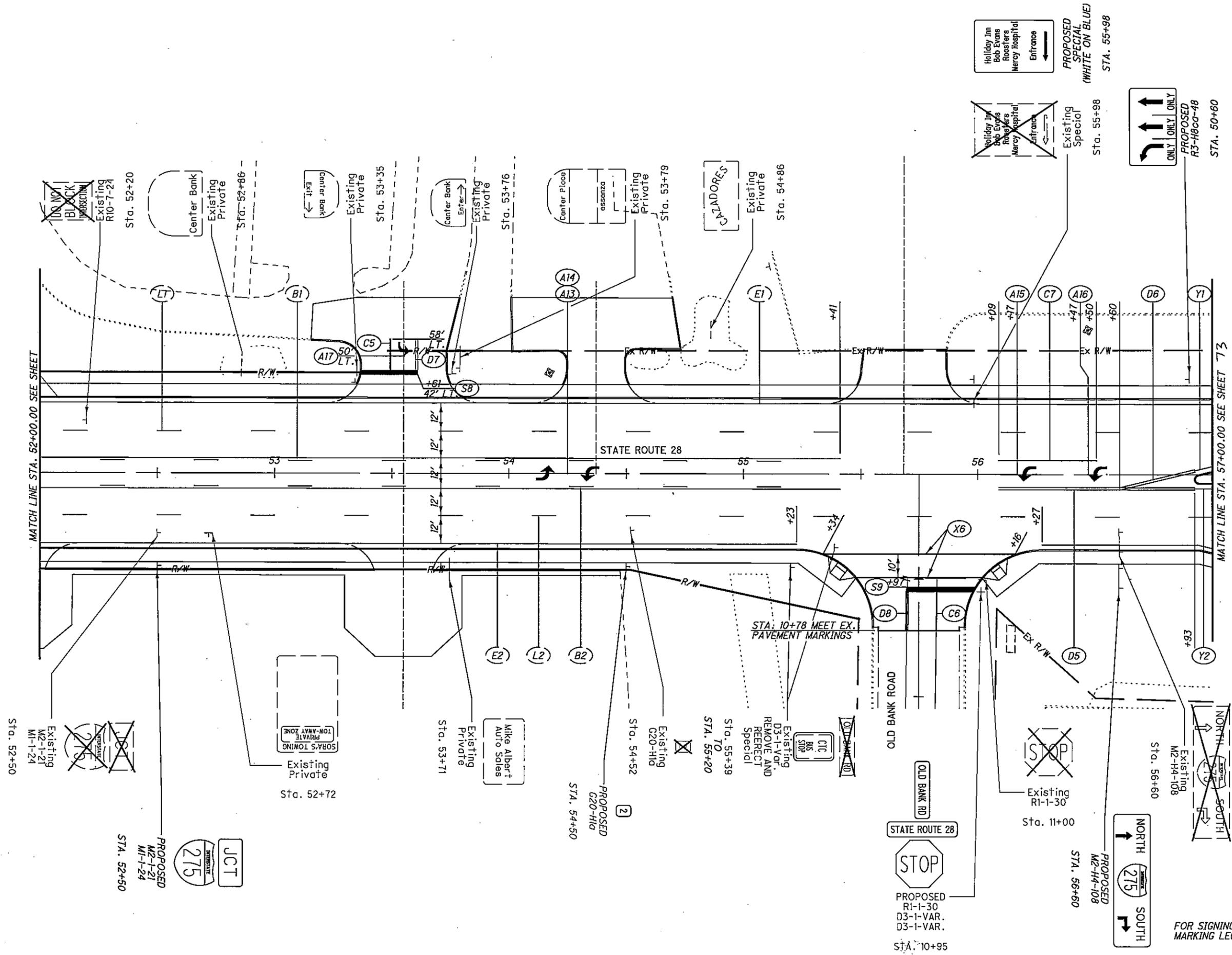
**TRAFFIC CONTROL PLAN
STA. 42+00.00 TO STA. 47+00.00**

CLE-28-2.05



FOR SIGNING AND PAVEMENT MARKING LEGENDS SEE LEFT THIS SHEET.

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FOR SIGNING AND PAVEMENT MARKING LEGENDS SEE SHEET 10

CALCULATED	BMH
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TRAFFIC CONTROL PLAN
STA. 52+00.00 TO STA. 57+00.00

CLE-28-2.05

17

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

WATER:

CLERMONT COUNTY SEWER DISTRICT
2379 CLERMONT CENTER DRIVE
BATAVIA, OHIO 45103
LYLE BLOOM
513-732-8860

CITY OF MILFORD
745 CENTER STREET
MILFORD, OHIO 45150
MATTHEW NEWMAN
513-831-6819

SANTIARY:

CITY OF MILFORD
745 CENTER STREET
MILFORD, OHIO 45150
BUD WHITE
513-248-5098

TELECOMMUNICATIONS:

CINCINNATI BELL TELEPHONE
221 EAST FOURTH STREET
BUILDING 343
CINCINNATI, OHIO 45201-2301
MARK CONNOR
513-565-7043

TIME WARNER CABLE
11252 CORNELL PARK DRIVE
CINCINNATI, OHIO 45202
GARY NAPIER
513-469-5483

GAS:

DUKE ENERGY
139 EAST FOURTH STREET
ROOM 460A
CINCINNATI, OHIO 45202
BILL HOCTOR
513-287-2916

ELECTRIC:

DUKE ENERGY
139 EAST FOURTH STREET
ROOM 467 ANNEX
CINCINNATI, OHIO 45201
VIRGINIA MEYER
513-287-1748

ODOT DISTRICT 8
505 SOUTH SR 741
LEBANON, OHIO 45036
JIM JUDD
513-933-6692

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

LEGEND:

- WD = WARRANTY DEED
- SH = STANDARD HIGHWAY EASEMENT
- LA = LIMITED ACCESS EASEMENT
- T = TEMPORARY EASEMENT
- U = UTILITY EASEMENT

CONVENTIONAL SYMBOLS

County Line	-----	Ditch / Creek (Ex)	-----
Township Line	-----	Ditch / Creek (Pr)	-----
Section Line	-----	Tree Line (Ex)	-----
Corporation Line	----- or -----	Ownership Hook Symbol Z	Example Z
Fence Line (Ex)	----- (Pr)	Property Line Symbol P	Example P
Center Line	-----	Break Line Symbol V	Example V
Right of Way (Ex)	----- Ex R/W	Tree (Pr)	Tree (Ex) Shrub (Ex)
Right of Way (Pr)	----- R/W	Tree (Remove)	Shrub (Remove)
Standard Highway Ease. (Ex)	----- Ex SH	Evergreen (Ex)	Stump
Temporary Right of Way	----- TMP	Evergreen (Remove)	Stump (Remove)
Channel Ease. (Pr)	----- CH	Wetland (Pr)	Grass (Pr) Aerial Target
Utility Ease. (Ex)	----- Ex U	Post (Ex)	Mailbox (Ex) Mailbox (Pr)
Railroad	----- or -----	Light (Ex)	Telephone Marker (Ex) TEL
Guardrail (Ex)	----- (Pr)	Fire Hydrant (Ex)	Water Meter (Ex)
Construction Limits	-----	Water Valve (Ex)	Utility Valve Unknown (Ex)
Edge of Pavement (Ex)	-----	Telephone Pole (Ex)	Power Pole (Ex)
Edge of Pavement (Pr)	-----	Light Pole (Ex)	
Edge of Shoulder (Ex)	-----		
Edge of Shoulder (Pr)	-----		

RIGHT OF WAY LEGEND SHEET CLE-28-2.05

CLERMONT COUNTY
MIAMI TOWNSHIP
MILITARY SURVEY #597
CITY OF MILFORD

INDEX OF SHEETS:

LEGEND SHEET	1
CENTERLINE PLAT	2-3
PROPERTY MAP	4
SUMMARY OF ADDITIONAL R/W	5-7
R/W DETAIL	8-12

PLANS PREPARED BY:

FIRM NAME : LJB, INC.

PLANS PREPARED BY: ANDREW J. SHAHAN, HARRY G. HERBST III

FIELD REVIEW BY: _____

DATE COMPLETED: _____

OWNERSHIP VERIFIED BY: _____

DATE COMPLETED: _____

DATE COMPLETED: _____

I, ANDREW J. SHAHAN, P. S., HAVE CONDUCTED A SURVEY OF THE EXISTING CONDITIONS FOR THE OHIO DEPARTMENT OF TRANSPORTATION IN FEBRUARY/MARCH, 2008. THE RESULTS OF THAT SURVEY ARE CONTAINED HEREIN.

THE HORIZONTAL COORDINATES EXPRESSED HEREIN ARE GROUND COORDINATES BASED ON THE OHIO STATE PLANE COORDINATES SYSTEM, SOUTH ZONE, NAD83, (2007), AND WERE ESTABLISHED USING ODOT VRS. THE PROJECT ADJUSTMENT FACTOR USED FOR THIS PROJECT IS 1.0000866544. FURTHER MORE, I HAVE CALCULATED THE PROPOSED PROPERTY LINES, GROSS TAKE, PRESENT ROADWAY OCCUPIED (PRO), NET TAKE AND NET RESIDUE; AS WELL AS PREPARED THE LEGAL DESCRIPTIONS NECESSARY TO ACQUIRE THESE PARCELS AS SHOWN HEREIN.

AS A PART OF THIS PROJECT I HAVE DETERMINED THE LOCATIONS OF THE EXISTING PROPERTY LINES FOR PROPERTY TAKES CONTAINED HEREIN. I ALSO HAVE SET MONUMENTS AT THE PROPOSED PROPERTY CORNERS, SECTION CORNERS AND OTHER POINTS SHOWN HEREIN. THE CENTERLINE MONUMENTATION CALLED FOR HEREIN WILL BE SET UNDER MY DIRECT SUPERVISION DURING THE CONSTRUCTION OF THIS PROJECT. THIS WORK WILL BE DONE IN ACCORDANCE WITH OAC 4733-37 AS CITED BELOW.

ALL OF MY WORK CONTAINED HEREIN WAS CONDUCTED IN ACCORDANCE WITH OHIO ADMINISTRATIVE CODE 4733-37 COMMONLY KNOWN AS MINIMUM STANDARDS FOR BOUNDARY SURVEY IN THE STATE OF OHIO UNLESS SO NOTED.

THE WORDS I AND MY AS USED HEREIN ARE TO MEAN THAT EITHER MYSELF OR SOMEONE WORKING FOR ME UNDER MY DIRECT CONTROL OR SUPERVISION.

DATE: _____

ANDREW J. SHAHAN, OHIO PROFESSIONAL LAND SURVEYOR NO. 8378

SURVEYORS SEAL



SIGNED: _____
DATE: _____

FEDERAL PROJECT NO. E070489

PID NO. 82140

CALCULATED HGH CHECKED AJU

RIGHT OF WAY LEGEND SHEET

CLE-28-2.05

1 / 12

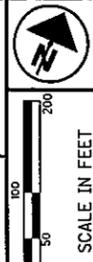
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CLERMONT COUNTY
MIAMI TOWNSHIP
MILITARY SURVEY #597
CITY OF MILFORD

RECEIVED _____, 20____
RECORDED _____, 20____
BOOK _____ PAGE _____

COUNTY RECORDER



PID NO. 82140

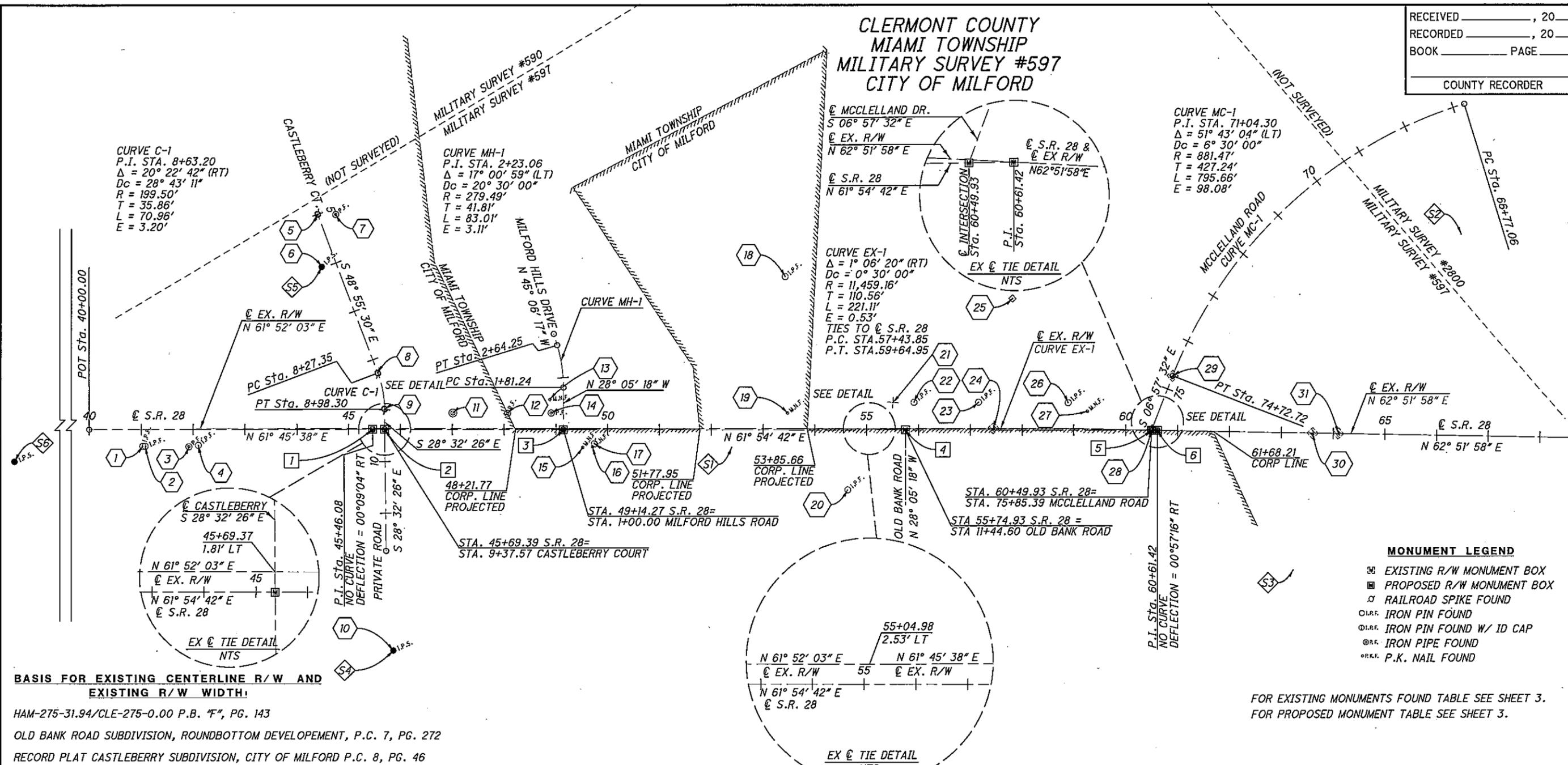
R/W DESIGNER HIGH R/W REVIEWER AUS

CENTERLINE PLAT

CLE-28-2.05

2/12

83



BASIS FOR EXISTING CENTERLINE R/W AND EXISTING R/W WIDTH:

- HAM-275-31.94/CLE-275-0.00 P.B. "F", PG. 143
- OLD BANK ROAD SUBDIVISION, ROUNDBOTTOM DEVELOPEMENT, P.C. 7, PG. 272
- RECORD PLAT CASTLEBERRY SUBDIVISION, CITY OF MILFORD P.C. 8, PG. 46
- PLAT OF MILFORD HILLS SUBDIVISION, P.B. "G", PG. 69

BASIS FOR BEARINGS:

TO OBTAIN A "MODIFIED" STATE PLANE COORDINATE SYSTEM WITH GROUND MEASUREMENTS, VRS WAS UTILIZED TO ESTABLISH CONTROL POINTS S1, S2, S3, S4, S5, & S6 FOR HORIZONTAL CONTROL. THE HORIZONTAL DATUM IS NAD83 IN THE OHIO SOUTH 3402 ZONE. THE GRID TO GROUND PROJECTION WAS SCALED AT LATITUDE 39° 11' 03.06910" N, LONGITUDE 84° 16' 06.81159" W AND HEIGHT 599.885 SFT. THE GROUND SCALE FACTOR IS 1.0000866544. GEOID03 (CONUS) WAS THE MODEL USED WITH THE WGS84 ELLIPSOID. TO ESTABLISH VERTICAL CONTROL, SAID CONTROL POINT S1 HAVING AN ELEVATION OF 702.58 WAS HELD. SPIRIT LEVELS WERE RUN HOLDING SAID CONTROL POINT S1 THROUGH CONTROL POINTS S2, S3, S4, S5 & S6, CONDUCTING A VERTICAL CALIBRATION ON SAID CONTROL POINTS PRODUCING AN MAXIMUM VERTICAL RESIDUAL ERROR OF 0.046'.

ALL COORDINATES GIVEN ARE GROUND COORDINATES.

THE PROPOSED RIGHT OF WAY SHALL BE REFERENCED FROM THE CENTERLINE OF RIGHT OF WAY.

CENTERLINE OF CONSTRUCTION SHALL BE CENTERLINE OF RIGHT OF WAY.

THE PLACING OF MONUMENTS SHALL BE UNDER THE DIRECTION OF A SURVEYOR REGISTERED IN THE STATE OF OHIO. THE CENTERLINE MONUMENT ASSEMBLY BOX(S) WILL BE INSTALLED BY THE HIGHWAY CONTRACTOR AT THE TIME OF CONSTRUCTION. THE IRON PIN WITH CAP MARKING THE ACTUAL CENTERLINE STATION POINT AND ANY REFERENCE MONUMENTS ARE TO BE SET BY THE RIGHT OF WAY DESIGN CONSULTANT.

CHANGES OR ALTERATIONS TO THE LOCATION OF ANY MONUMENTS SHOWN ON THIS PLAT, REQUIRES PRIOR APPROVAL OF THE DISTRICT REAL ESTATE ADMINISTRATOR OF THE OHIO DEPARTMENT OF TRANSPORTATION. A REVISED CENTERLINE PLAT WITH THE NEW LOCATIONS SHALL BE RECORDED IN THE APPLICABLE COUNTY RECORDS AND THE OHIO DEPARTMENT OF TRANSPORTATION. SPECIFICATIONS FOR CENTERLINE MONUMENTS, REFERENCE MONUMENTS AND RIGHT OF WAY MONUMENTS ARE SHOWN ON STANDARD CONSTRUCTION DRAWING RM-1.1 OF THE OHIO DEPARTMENT OF TRANSPORTATION.

- MONUMENT LEGEND**
- EXISTING R/W MONUMENT BOX
 - PROPOSED R/W MONUMENT BOX
 - RAILROAD SPIKE FOUND
 - IRON PIN FOUND
 - IRON PIN FOUND W/ ID CAP
 - IRON PIPE FOUND
 - P.K. NAIL FOUND

FOR EXISTING MONUMENTS FOUND TABLE SEE SHEET 3.
FOR PROPOSED MONUMENT TABLE SEE SHEET 3.

I, ANDREW J. SHAHAN, P. S., HAVE CONDUCTED A SURVEY OF THE EXISTING CONDITIONS FOR THE OHIO DEPARTMENT OF TRANSPORTATION IN FEBRUARY/MARCH, 2008. THE RESULTS OF THAT SURVEY ARE CONTAINED HEREIN.

THE HORIZONTAL COORDINATES EXPRESSED HEREIN ARE GROUND COORDINATES BASED ON THE OHIO STATE PLANE COORDINATES SYSTEM, SOUTH ZONE, NAD83, (2007), AND WERE ESTABLISHED USING ODOT VRS. THE PROJECT ADJUSTMENT FACTOR USED FOR THIS PROJECT IS 1.0000866544. FURTHER MORE, I HAVE CALCULATED THE PROPOSED PROPERTY LINES, GROSS TAKE, PRESENT ROADWAY OCCUPIED (PRO), NET TAKE AND NET RESIDUE; AS WELL AS PREPARED THE LEGAL DESCRIPTIONS NECESSARY TO ACQUIRE THESE PARCELS AS SHOWN HEREIN.

AS A PART OF THIS PROJECT I HAVE DETERMINED THE LOCATIONS OF THE EXISTING PROPERTY LINES FOR PROPERTY TAKES CONTAINED HEREIN. I ALSO HAVE SET MONUMENTS AT THE PROPOSED PROPERTY CORNERS, SECTION CORNERS AND OTHER POINTS SHOWN HEREIN. THE CENTERLINE MONUMENTATION CALLED FOR HEREIN WILL BE SET UNDER MY DIRECT SUPERVISION DURING THE CONSTRUCTION OF THIS PROJECT. THIS WORK WILL BE DONE IN ACCORDANCE WITH OAC 4733-37 AS CITED BELOW.

ALL OF MY WORK CONTAINED HEREIN WAS CONDUCTED IN ACCORDANCE WITH OHIO ADMINISTRATIVE CODE 4733-37 COMMONLY KNOWN AS MINIMUM STANDARDS FOR BOUNDARY SURVEY IN THE STATE OF OHIO UNLESS SO NOTED.

THE WORDS I AND MY AS USED HEREIN ARE TO MEAN THAT EITHER MYSELF OR SOMEONE WORKING FOR ME UNDER MY DIRECT CONTROL OR SUPERVISION.

ANDREW J. SHAHAN, OHIO PROFESSIONAL LAND SURVEYOR NO. 8378

DATE: _____

SURVEYORS SEAL



SIGNED: _____
DATE: _____

EXISTING MONUMENTATION FOUND

EXISTING MONUMENT	FOUND	STATION	OFFSET	NORTHING	EASTING
①	5/8" IRON PIN, "G S NICHOLS"	41+03.01	32.39' RT	435671.0220	1466227.6730
②	5/8" IRON PIN, "G S NICHOLS"	41+07.46	32.25' RT	435673.2560	1466231.5230
③	1" IRON PIPE	41+91.74	33.38' RT	435712.1340	1466306.3120
④	1/2" IRON PIN	42+09.95	30.67' RT	435723.1430	1466321.0770
⑤	RAILROAD SPIKE	44+40.70	414.92' LT	436224.8740	1466313.5260
⑥	5/8" IRON PIN, CAP "TRAVERSE POINT"	44+48.53	313.85' LT	436139.5380	1466368.2430
⑦	5/8" IRON PIPE	44+75.67	414.60' LT	436241.1410	1466344.4800
⑧	RAILROAD SPIKE	45+56.04	108.70' LT	436009.7930	1466560.2950
⑨	RAILROAD SPIKE	45+69.08	39.33' LT	435954.7310	1466604.4550
⑩	5/8" IRON PIN, CAP "TRAVERSE POINT"	45+86.61	427.58' RT	435551.0660	1466839.7550
⑪	CONCRETE MONUMENT, KNOCKED OVER	47+00.78	31.91' LT	436010.1920	1466724.1350
⑫	5/8" IRON PIPE	48+06.50	31.43' LT	436059.5460	1466817.6320
⑬	MAG NAIL	48+87.93	57.98' LT	436121.3070	1466876.9770
⑭	5/8" IRON PIPE, BENT SOUTH	48+90.49	32.06' LT	436099.6429	1466891.4339
⑮	MAG NAIL	49+51.78	34.61' RT	436069.6880	1466976.8980
⑯	MAG NAIL	49+70.55	33.48' RT	436079.5170	1466992.9250

EXISTING MONUMENTATION FOUND

EXISTING MONUMENT	FOUND	STATION	OFFSET	NORTHING	EASTING
⑰	RAILROAD SPIKE	49+74.02	27.78' RT	436086.1860	1466993.3040
⑱	5/8" IRON PIN, BENT EAST	53+42.64	295.74' LT	436545.1570	1467166.1820
⑲	MAG NAIL	53+47.30	32.41' LT	436315.0350	1467294.2790
⑳	5/8" IRON PIN, BENT NORTH	54+63.81	115.80' RT	436239.1360	1467466.8450
㉑	CROSS CUT	55+50.93	52.99' LT	436429.0730	1467464.2350
㉒	5/8" IRON PIN, "FELDBUSCH"	55+91.35	52.76' LT	436447.8921	1467500.0005
㉓	5/8" IRON PIN, "FELDBUSCH"	57+15.77	53.08' LT	436506.7670	1467609.6205
㉔	⊕ MONUMENT BOX	57+44.30	3.25' LT	436476.2320	1467658.2500
㉕	HUB AT PROP CORNER	57+80.76	253.49' LT	436714.1720	1467572.5960
㉖	5/8" IRON PIN, BENT NORTH	58+89.30	52.56' LT	436588.0093	1467762.9578
㉗	MAG NAIL	59+26.31	36.44' LT	436591.2070	1467803.1980
㉘	CONCRETE MONUMENT	60+50.25	0.19' LT	436617.5830	1467929.6090
㉙	8"X8" MONUMENT BOX, TOP BROKEN	60+88.79	105.57' LT	436729.1060	1467915.7620
㉚	8"X8" MONUMENT BOX	63+60.06	0.12' RT	436758.7760	1468205.3780
㉛	8"X8" MONUMENT BOX	64+09.65	ON ⊕	436781.4950	1468249.4560

MONUMENT ASSEMBLY BOXES SET

⊕ MONUMENT	SET	STATION	OFFSET	GROUND COORDINATES	
				NORTHING	EASTING
1	MONUMENT ASSEMBLY	45+46.08	ON ⊕	435909.2045	1466602.6841
2	MONUMENT ASSEMBLY	45+69.39	ON ⊕	435920.1787	1466623.2471
3	MONUMENT ASSEMBLY	49+14.27	ON ⊕	436082.5589	1466927.5064
4	MONUMENT ASSEMBLY	55+74.93	ON ⊕	436393.6189	1467510.3539
5	MONUMENT ASSEMBLY	60+49.93	ON ⊕	436617.2664	1467929.4126
6	MONUMENT ASSEMBLY	60+61.42	ON ⊕	436622.6783	1467939.5531

CONTROL POINTS

NUMBER	CONTROL POINT SET	STATION	OFFSET	STATE PLANE GROUND COORDINATES	
				NORTHING	EASTING
Ⓢ1	MAG NAIL	52+39.45	33.44 RT	436206.1680	1467230.1310
Ⓢ2	5/8" IRON PIN, CAP "TRAVERSE POINT"	66+40.65	388.65 LT	437232.7280	1468277.7820
Ⓢ3	5/8" IRON PIN, CAP "TRAVERSE POINT"	63+28.09	264.32 RT	436509.0700	1468297.4210
Ⓢ4	5/8" IRON PIN, CAP "TRAVERSE POINT"	45+86.61	427.58' RT	435551.0660	1466839.7550
Ⓢ5	5/8" IRON PIN, CAP "TRAVERSE POINT"	44+40.70	414.92' LT	436139.5380	1466368.2430
Ⓢ6	5/8" IRON PIN, CAP "TRAVERSE POINT"	OUT OF ALIGNMENT RANGE		435257.4130	1465519.8840

I, ANDREW J. SHAHAN, P. S., HAVE CONDUCTED A SURVEY OF THE EXISTING CONDITIONS FOR THE OHIO DEPARTMENT OF TRANSPORTATION IN FEBRUARY/MARCH, 2008. THE RESULTS OF THAT SURVEY ARE CONTAINED HEREIN.

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THE WORDS I AND MY AS USED HEREIN ARE TO MEAN THAT EITHER MYSELF OR SOMEONE WORKING FOR ME UNDER MY DIRECT CONTROL OR SUPERVISION.

DATE: _____

ANDREW J. SHAHAN, OHIO PROFESSIONAL LAND SURVEYOR NO. 8378

RECEIVED _____, 20____
 RECORDED _____, 20____
 BOOK _____ PAGE _____

COUNTY RECORDER

PID NO.

82140

R/W DESIGNER
 HIGH
 R/W REVIEWER
 AJS

CENTERLINE PLAT

CLE-28-2.05

3 / 12

84

SURVEYORS SEAL

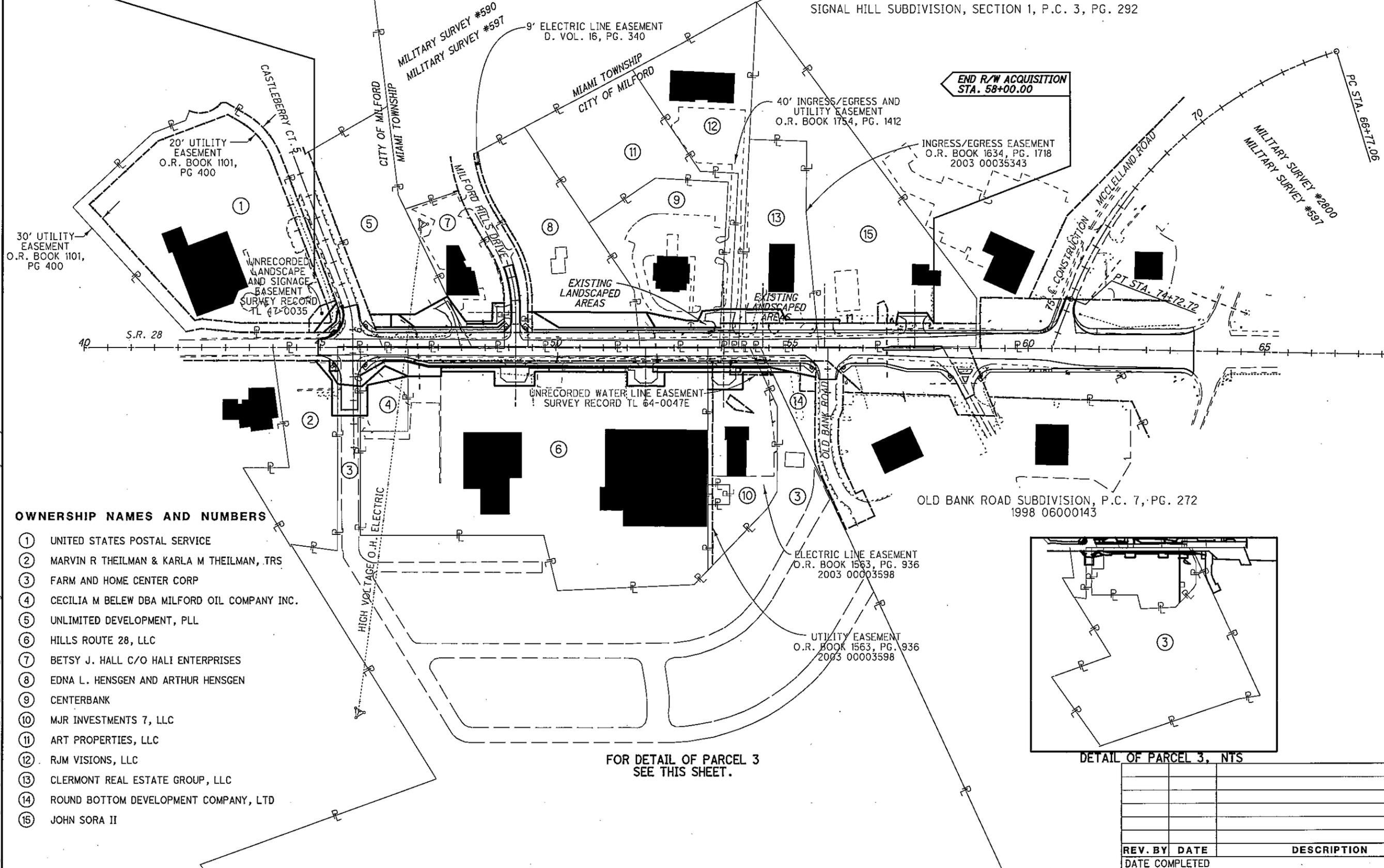


SIGNED: _____
 DATE: _____

CLERMONT COUNTY
MIAMI TOWNSHIP
MILITARY SURVEY #597
CITY OF MILFORD

BEGIN R/W ACQUISITION
STA. 44+87.44

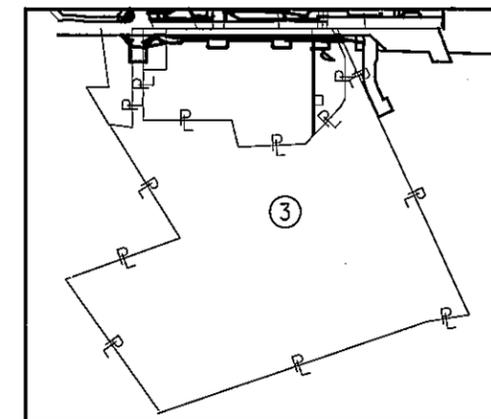
END R/W ACQUISITION
STA. 58+00.00



OWNERSHIP NAMES AND NUMBERS

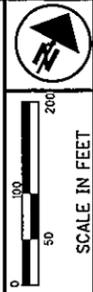
- ① UNITED STATES POSTAL SERVICE
- ② MARVIN R THEILMAN & KARLA M THEILMAN, TRS
- ③ FARM AND HOME CENTER CORP
- ④ CECILIA M BELEW DBA MILFORD OIL COMPANY INC.
- ⑤ UNLIMITED DEVELOPMENT, PLL
- ⑥ HILLS ROUTE 28, LLC
- ⑦ BETSY J. HALL C/O HALI ENTERPRISES
- ⑧ EDNA L. HENSGEN AND ARTHUR HENSGEN
- ⑨ CENTERBANK
- ⑩ MJR INVESTMENTS 7, LLC
- ⑪ ART PROPERTIES, LLC
- ⑫ RJM VISIONS, LLC
- ⑬ CLERMONT REAL ESTATE GROUP, LLC
- ⑭ ROUND BOTTOM DEVELOPMENT COMPANY, LTD
- ⑮ JOHN SORA II

FOR DETAIL OF PARCEL 3
SEE THIS SHEET.



DETAIL OF PARCEL 3, NTS

REV. BY	DATE	DESCRIPTION



PID NO. **82140**
R/W DESIGNER: HIGH
R/W REVIEWER: AUS

PROPERTY MAP

CLE-28-2.05

4/12
85

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TOTAL NUMBER OF: OWNERSHIPS 15 OWNERSHIPS WITH STRUCTURES INVOLVED 0 GRantee: ALL RIGHT OF WAY ACQUIRED IN THE NAME OF THE BOARD OF COUNTY COMMISSIONERS OF CLERMONT COUNTY, OHIO UNLESS OTHERWISE SHOWN.
 PARCELS 42
 TOTAL TAKES 0 ALL AREAS IN ACRES NET RESIDUE = RECORD AREA - TOTAL PRO - NET TAKE * DENOTES RIGHT OF WAY ENCROACHMENT

PARCEL NO.	OWNER	SHEET NO.	OFFICIAL RECORD		AUDITOR'S PARCEL	RECORD AREA	TOTAL P.R.O.	GROSS TAKE	P.R.O. IN TAKE	NET TAKE	STRUC-TURE	NET RESIDUE		TYPE FUND	REMARKS AND PERSONALTY	AS ACQUIRED	
			VOL	PAGE								LEFT	RIGHT			BOOK	PAGE
1-WDV	UNITED STATES POSTAL SERVICE	8	1101	400	210740.248P	4.005	0										
1-T															GRADING		
2-WDV	MARVIN R. THEILMAN & KARLA M. THEILMAN, TRUSTEES	8	1275	947	210741.028	1.08	0.094								* COMMERCIAL SIGN		
2-SW															GRADING		
2-T																	
3-WDV	FARM AND HOME CENTER COMPANY, AN OHIO CORPORATION	8, 10	D.B. 316, PG. 441		210741.029	26.37	0.049										
3-SW															GRADING		
3-T1															CURB IN PARKING LOT		
3-T2																	
4-WDV	CECILIA BELEW dba MILFORD OIL COMPANY	8	D.B. 695, PG. 841		210741.293	0.41	0.069								* CONCRETE BASE		
4-SW															GRADING.		
4-T																	
5-WDV	UNLIMITED DEVELOPMENT, P.L.L.	8, 9	1764	108	210740.125P	1.8	0.142										
5-SW															GRADING		
5-T1															GRADING		
5-T2																	
6-WDV	HILLS ROUTE 28, LLC	8, 9, 10	1735	278	210741.278	7.178	0.418								* 18 PARKING SPACES, COMMERCIAL SIGN		
6-SW1																	
6-SW2																	
6-SW3																	
6-T1															30 DAY DURATION; DRIVE AND PARKING LOT REGRADING		
6-T2															30 DAY DURATION; DRIVE AND PARKING LOT REGRADING		

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FEDERAL PROJECT NO. E070489
 PID NO. 82140
 STATE JOB NO.
 R/W DESIGNER HIGH
 R/W REVIEWER AJ5
 SUMMARY
 OF ADDITIONAL RIGHT OF WAY
 CLE-28-2.05
 5/12
 86

NOTE: ALL TEMPORARY PARCELS TO BE OF 12 MONTHS DURATION UNLESS NOTED OTHERWISE.

NOTE: UNDER NO CIRCUMSTANCES ARE TEMPORARY EASEMENTS TO BE USED FOR STORAGE OF MATERIAL OR EQUIPMENT BY THE CONTRACTOR UNLESS NOTED OTHERWISE.

(C) = CALCULATED AREA

LEGEND: WDV - FEE SIMPLE RESERVATION OF ACCESS ACQUIRED IN THE NAME OF ANOTHER AGENCY.
 SW - STORM SEWER EASEMENT
 T - TEMPORARY EASEMENT

REV. BY	DATE	DESCRIPTION
FIELD REVIEW BY:	DATE	
OWNERSHIP VERIFIED BY:	DATE	
DATE COMPLETED		

TOTAL NUMBER OF: OWNERSHIPS 15, PARCELS 42, TOTAL TAKES 0. OWNERSHIPS WITH STRUCTURES INVOLVED 0. GRANTEE: ALL RIGHT OF WAY ACQUIRED IN THE NAME OF THE BOARD OF COUNTY COMMISSIONERS OF CLERMONT COUNTY, OHIO UNLESS OTHERWISE SHOWN. ALL AREAS IN ACRES. NET RESIDUE = RECORD AREA - TOTAL PRO - NET TAKE. * DENOTES RIGHT OF WAY ENCROACHMENT.

PARCEL NO.	OWNER	SHEET NO.	OFFICIAL RECORD		AUDITOR'S PARCEL	RECORD AREA	TOTAL P.R.O.	GROSS TAKE	P.R.O. IN TAKE	NET TAKE	STRUC-TURE	NET RESIDUE		TYPE FUND	REMARKS AND PERSONALTY	AS ACQUIRED	
			VOL	PAGE								LEFT	RIGHT			BOOK	PAGE
7-WDV	BETSY J. HALL	9	827	275	182513D288	0.81	0.059										
7-T															30 DAY DURATION; DRIVE AND PARKING LOT REGRADING		
8-WDV	EDNA L. HENSGEN AND ARTHUR HENSGEN	9	2166 D.B. 453, PG. 31	1118 D.B. 311, PG. 132	182513D040	2.87	0.164										
8-T															GRADING		
9-WDV	CENTERBANK	9, 10	1252	1582	210740.292P	1.742	0.126								* COMMERCIAL SIGN, ROCK AND LANDSCAPING		
9-SW																	
9-T															GRADING AND DRIVEWAY		
10-WDV	M.J.R. INVESTMENTS 7, LLC	10	1563	936	210741.294	1.244	0.064								* COMMERCIAL SIGN		
10-SW																	
10-T															30 DAY DURATION; DRIVE AND PARKING LOT REGRADING		
11-WDV	ART PROPERTIES, LLC	10	2017	584	210740.293P	1.625	0.014								* COMMERCIAL SIGN		
11-T															DRIVEWAY		
12-WDV	RJM VISIONS, LLC	10	1754	1411	210740.042P	1.468	0.014								* COMMERCIAL SIGN		
12-T															DRIVEWAY		

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NOTE: ALL TEMPORARY PARCELS TO BE OF 12 MONTHS DURATION UNLESS NOTED OTHERWISE.

NOTE: UNDER NO CIRCUMSTANCES ARE TEMPORARY EASEMENTS TO BE USED FOR STORAGE OF MATERIAL OR EQUIPMENT BY THE CONTRACTOR UNLESS NOTED OTHERWISE.

(C.A.) CALCULATED AREA

LEGEND: WDV - FEE SIMPLE RESERVATION OF ACCESS ACQUIRED IN THE NAME OF ANOTHER AGENCY.
SW - STORM SEWER EASEMENT
T - TEMPORARY EASEMENT

REV. BY	DATE	DESCRIPTION
FIELD REVIEW BY:	DATE	
OWNERSHIP VERIFIED BY:	DATE	
DATE COMPLETED		

FEDERAL PROJECT NO. E070489
PID NO. 82140
STATE JOB NO.
R/W DESIGNER HIGH
R/W REVIEWER AJS
SUMMARY OF ADDITIONAL RIGHT OF WAY
CLE-28-2.05
6/12
87

TOTAL NUMBER OF: OWNERSHIPS 15 OWNERSHIPS WITH STRUCTURES INVOLVED 0 GRANTEE: ALL RIGHT OF WAY ACQUIRED IN THE NAME OF THE BOARD OF COUNTY COMMISSIONERS OF CLERMONT COUNTY, OHIO UNLESS OTHERWISE SHOWN.
 PARCELS 42
 TOTAL TAKES 0 ALL AREAS IN ACRES NET RESIDUE = RECORD AREA - TOTAL PRO - NET TAKE * DENOTES RIGHT OF WAY ENCROACHMENT

PARCEL NO.	OWNER	SHEET NO.	OFFICIAL RECORD		AUDITOR'S PARCEL	RECORD AREA	TOTAL P.R.O.	GROSS TAKE	P.R.O. IN TAKE	NET TAKE	STRUC-TURE	NET RESIDUE		TYPE FUND	REMARKS AND PERSONALTY	AS ACQUIRED	
			VOL	PAGE								LEFT	RIGHT			BOOK	PAGE
13-WDV	CLERMONT REAL ESTATE GROUP, LLC	10	1634	1718	182513D047	1.405	0.192								* LANDSCAPING		
13-T															DRIVEWAY * ROCK AND LANDSCAPING		
14-WDV	ROUND BOTTOM DEVELOPMENT COMPANY, LTD	10	1122	667	210741.284	0.2757	0.000								* CONCRETE PAD		
14-T															GRADING		
15-T1	JOHN SORA II	10, 11	859 2099	663 1533	182513D043	3.372	0.390								DRIVEWAY		
15-T2															DRIVEWAY		

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FEDERAL PROJECT NO. E070489
 PID NO. 82140
 STATE JOB NO.
 R/W DESIGNER HIGH
 R/W REVIEWER AJS
 SUMMARY OF ADDITIONAL RIGHT OF WAY
 CLE-28-2.05
 7 / 12
 88

NOTE: ALL TEMPORARY PARCELS TO BE OF 12 MONTHS DURATION UNLESS NOTED OTHERWISE.

NOTE: UNDER NO CIRCUMSTANCES ARE TEMPORARY EASEMENTS TO BE USED FOR STORAGE OF MATERIAL OR EQUIPMENT BY THE CONTRACTOR UNLESS NOTED OTHERWISE.

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LEGEND: WDV - FEE SIMPLE RESERVATION OF ACCESS ACQUIRED IN THE NAME OF ANOTHER AGENCY.
 SW - STORM SEWER EASEMENT
 T - TEMPORARY EASEMENT

REV. BY	DATE	DESCRIPTION
FIELD REVIEW BY:	DATE	
OWNERSHIP VERIFIED BY:	DATE	
DATE COMPLETED		

* DENOTES R/W ENCROACHMENT.

CLERMONT COUNTY MIAMI TOWNSHIP CITY OF MILFORD MILITARY SURVEY 597

CASTLEBERRY COURT CURVE DATA
P.I. STA. 8+63.20
 $\Delta = 20^\circ 22' 42''$ (RT)
 $D_c = 28^\circ 43' 11''$
 $R = 199.50'$
 $T = 35.86'$
 $L = 70.96'$
 $E = 3.20'$
 $e_{max} = NC$

UNITED STATES
POSTAL SERVICE
O.R. VOL 1101, PG 400
4.005 ACRES
210740.248P

ASPHALT
PARKING LOT
**BEGIN R/W ACQUISITION
STA. 44+87.44**

**BEGIN WORK
STA. 44+60.00**

**BEGIN PROJECT
STA. 46+19.53
S.L.M. =**

UNLIMITED DEVELOPMENT, PLL
O.R. VOL 1764, PG 108
1.8 ACRES
210740.125P

100 CASTLEBERRY CT.
1 STY BRICK/CONCRETE

CONCRETE WALK

20' UTILITY EASEMENT
SURVEY RECORD
TL-47-0035

PT STA. 8+98.30

EX. R/W
N 61° 52' 03" E

CONSTRUCTION S.R. 28
N 61° 54' 42" E

N 61° 45' 38" E

ELEC. MH

44+90.02
28.10' RT

45+20.00
28.16' RT

45+20.00
50.00' RT

45+37.23
50.00' RT

45+25.00
70.00' RT

45+37.18
77.26' RT

45+37.46
77.71' RT

45+85.57
82.25' RT

45+85.56
59.09' RT

45+85.53
28.18' RT

45+07.10
31.86' LT

45+18.54
31.84' LT

45+34.95
94.23' LT

45+38.50
82.40' LT

45+42.97
59.30' LT

45+33.60
54.46' LT

46+15.93
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46+20.18
46.31.85' LT

46+05.66
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46+50.00
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46+85.53
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46+78.94
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46+37.22
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46+85.56
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46+00.00
80.00' RT

45+88.73
82.55' RT

45+85.62
145.01' RT

45+25.00
145.00' RT

45+37.05
145.02' RT

45+85.62
145.01' RT

46+00.00
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46+85.56
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31.86' LT

45+18.54
31.84' LT

SIGNAL HILL SUBDIVISION, SECTION 1, P.C. 3, PG. 292

ASPHALT
PARKING
LOT

ASPHALT
PARKING
LOT

SIGNAL HILL PARTNERSHIP
182513B229
MCCELLELAND RD

MAIL ADDRESS:
SIGNAL HILL PARTNERSHIP
C/O US BANK/CORPORATE REAL ESTATE
FACILITIES MGMT-CINTI EAST
7350 BEECHMONT AVE
CINCINNATI OH 45230

CLERMONT COUNTY
MIAMI TOWNSHIP
CITY OF MILFORD
MILITARY SURVEY 597

END WORK
STA. 63+60.00

STATE OF OHIO
182513B220.

CONSTRUCTION LIMITS

63+50.00
43.11' LT

CONSTRUCTION, S.R. 28
N 62° 51' 58" E

63+50.00
47.68' RT

STATE OF OHIO
182513B219

MIAMI TOWNSHIP
CITY OF MILFORD

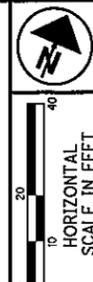
BEF REIT INC
O.R. VOL 1225, PG 1414
INST NO. 1999 00038250
210741.295
1.154 ACRES

OLD BANK ROAD SUBDIVISION
P.C. 7, PG. 272

ENTRANCE RAMP

EXIT RAMP

DIMENSIONS ARE SHOWN TO FACE OF CURB
UNLESS OTHERWISE NOTED.



PID NO.
82140

R/W DESIGNER
HIGH
R/W REVIEWER
AJS

RIGHT OF WAY PLAN
STA. 62+00.00 TO STA. 63+50.00

CLE-28-2.05

12 / 12

93

REV. BY	DATE	DESCRIPTION

I:\B\HIGHWAYS\B\Dat\B\CC\TID\B82140\B\row\sheet\B82140R\B005.dgn 7/14/2009 7:02:23 AM jdelaney



ljbinc.com